

AMENDMENT NO. 21
TO THE
OFFICIAL PLAN (2014)
FOR THE
CORPORATION OF THE TOWN OF PELHAM

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PART "A" – THE PREAMBLE

SECTION 1

TITLE AND COMPONENTS

This document was approved in accordance with Section 17 and 21 of the *Planning Act, R.S.O. 1990*, as amended and shall be known as Amendment No. 21 to the Official Plan adopted by By-law No. 3259 (2012) and confirmed by the Ontario Municipal Board decision of July 18, 2014, for the Town of Pelham Planning Area.

Part "A", the Preamble does not constitute part of this amendment.

Part "B", the Amendment, consisting of the following text, appendices and the following mapping schedules constitutes Amendment No. 21 to the Official Plan of the Town of Pelham:

Schedule A7: Neighbourhood Structure Plan;
Schedule A8: Greenlands Structure Plan;
Schedule A9: Street Structure Plan;
Schedule A10: Land Use Structure Plan; and
Schedule A11: Official Plan Designations

SECTION 2

PURPOSE OF THIS AMENDMENT

The purpose of this Official Plan Amendment is to refine and, in some cases, change the land use designation of certain lands shown on Schedule A10, attached hereto under Part "B".

The Town has initiated this Secondary Plan in response to the parent policies of its Official Plan, specifically Section B1.1.10: Fenwick which outlines the specific requirement for the preparation of a Secondary Plan in this area and the specific issues which are to be addressed through the Secondary Plan process.

The policies contained within this Secondary Plan reinforce provincial and regional policy directives of the Provincial Policy Statement, Places to Grow and the Region of Niagara's Official Plan. While a "complete community" vision is often the goal and focus of a Secondary Plan, community demographics and lifestyles can also influence decisions concerning land use, together with other more localized constraints and opportunities.

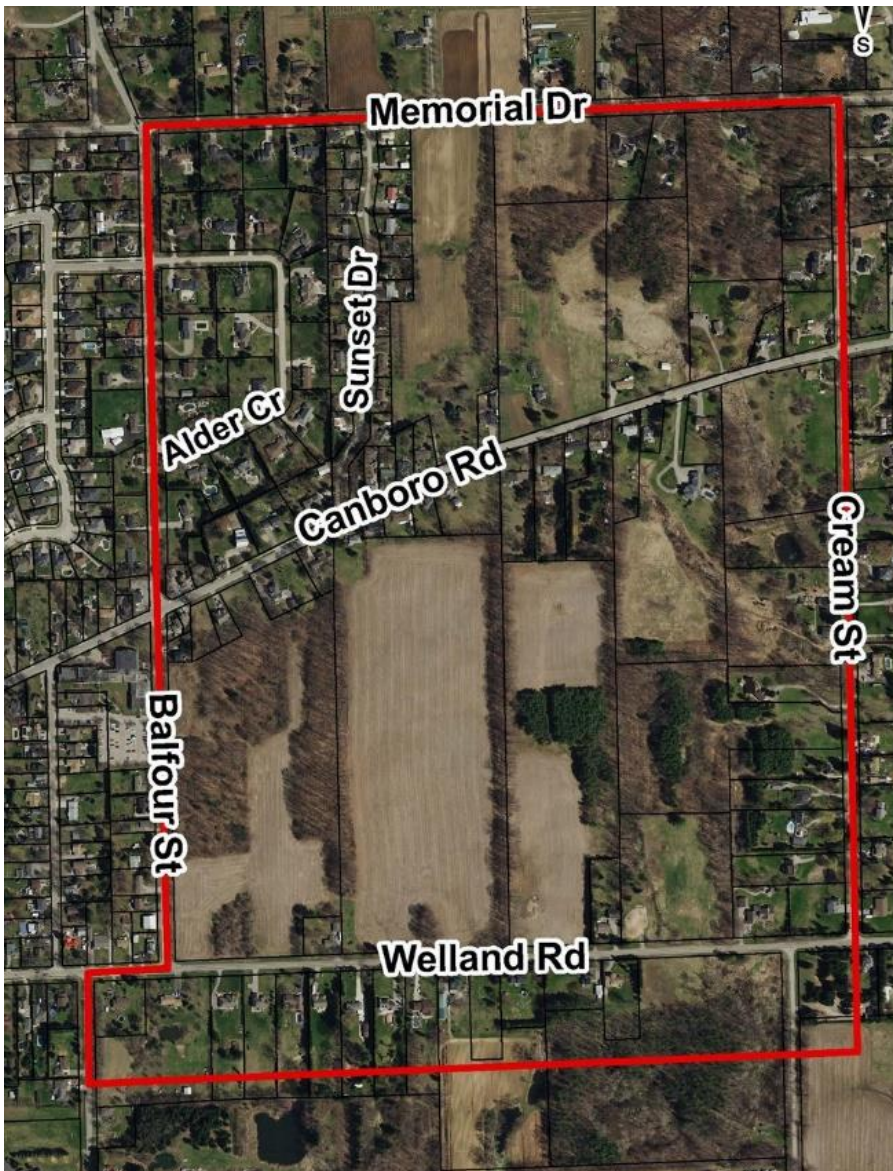
The Secondary Plan is intended to guide the future growth and development in East Fenwick and provides the framework to permit new development which is compatible with the character of Fenwick and consistent with Provincial, Regional and Local planning policies. The goal of the Secondary Plan is establish appropriate land use designations and policies that will support the future development of East Fenwick, establish design

guidelines for buildings, parks and streets, establish a system of public space areas and linkages with natural heritage areas and to identify existing transportation and infrastructure available in and around the study area and identify any upgrades that may be required to support the Secondary Plan.

SECTION 3

LOCATION OF THE AMENDMENT

The subject lands are located within the Urban Boundary of Fenwick. It is noted that these lands have been in the urban area boundary for Fenwick since the 1980's and the long-term intent has been that they be developed for urban use. The area is generally bounded by Balfour Street to the west, Welland Road to the south, Cream Street to the east and Memorial Drive to the North and is depicted in the aerial image below:



SECTION 4

BACKGROUND AND BASIS OF THE AMENDMENT

1. The *Planning Act, R.S.O. 1990*, as amended, provides that amendments may be made to the Official Plan. Policies of the Official Plan have been considered in the preparation of this Amendment and the following factors:
 - a) The policies will ensure compatibility with the surrounding land uses; and
 - b) This Amendment is consistent with the *Provincial Policy Statement* and conforms to the Growth Plan for the Greater Golden Horseshoe and the Region of Niagara Official Plan.
2. Official Plan Amendment No. 21 (East Fenwick Secondary Plan) is consistent with the Official Plan for the Town of Pelham, the Region of Niagara Official Plan, the Planning Act, Provincial Policy Statement, and A Place to Grow: Growth Plan for the Greater Golden Horseshoe as it sets out to provide for growth management and development over the long term:
 - a) Range of densities, with targeted intensification occurring in strategic locations throughout the plan area. Increased residential dwelling opportunities through low and medium density designations that will serve to assist in diversity and a range of affordability;
 - b) Age in place opportunities and choices are being enhanced; and
 - c) Utilizes urban lands and infrastructure efficiently through targeted intensification and development;
3. This Secondary Plan consolidates and addressed land use related issues at a neighbourhood scale, providing policy guidance for the neighbourhood through periods of short-term and long-range growth and development.
4. Section B1.1.10 Fenwick of the Official Plan for the Town of Pelham provides that the Town shall adopt a Secondary Plan for the East Fenwick area and amendments shall be made to the Official Plan to reflect the proposed land use changes. Subsection B1.1.10.1, B1.1.10.2 and B1.1.10.3 have been considered in support of this Official Plan Amendment.
5. The Secondary Plan reviews and addresses the following issues identified in Section B1.1.10.1 of the Town's Official Plan:
 - a) Conformity with the Provincial Growth Plan and Regional Official Plan. Specifically, the Secondary Plan identifies and permits a range of housing types and densities with the intent of achieving the Provincial requirement of 50 people and jobs per hectare;

- b) The preparation of a sub-watershed plan, prepared in accordance with the requirements of the Niagara Peninsula Conservation Authority and the Region of Niagara;
- c) Options for water, sanitary servicing and stormwater management;
- d) Review of overall hydrological and hydrogeological characteristics;
- e) The identification and protection of important natural heritage features;
- f) The preparation of a land use plan that addresses mix, density and phasing of proposed land uses in addition to identifying new or expanded parks and natural areas;
- g) The preparation of a conceptual transportation schedule including integration with existing roads and an assessment of pedestrian connections to trails and parklands;
- h) The preparation of a Stage 1 Archaeological Assessment; and
- i) The preparation of Urban Design Guidelines.

SECTION 5

IMPLEMENTATION AND INTERPRETATION

The relevant policies of the Official Plan adopted by By-law No. 3259 (2012) and confirmed by the Ontario Municipal Board decision of July 18, 2014, of the Town of Pelham Planning Area shall apply to the implementation and interpretation of this Amendment.

Approval of this Official Plan Amendment does not obligate Council in any way to fund any of the proposed public work items (road upgrades, trail networks, extension of servicing etc.) until such a time that these works have received budget approval through Council.

PART "B" – THE AMENDMENT

The Official Plan adopted by By-law No. 3259 (2012) and confirmed by the Ontario Municipal Board decision of July 18, 2014, for the Pelham Planning Area is hereby amended by adding the following:

B1.9 EAST FENWICK SECONDARY PLAN

B1.9.1 Introduction

- a) The lands subject to this Section of the Official Plan are identified on Schedule A7, Schedule A8, Schedule A9, Schedule A10 and Schedule A11, which are attached hereto and form part of this Official Plan;
- b) In addition to the identified Schedules and the following text, this Secondary Plan also includes the following appendices:
 - i. Appendix A – 2018 Neighbourhood Demonstration Plan and Active Transportation Plan
 - ii. Appendix B – 2018 Land Use Budget
 - iii. Appendix C – Urban Design Guidelines
 - iv. Appendix D – August 2019 Natural Heritage Mapping
 - v. Appendix E - Cultural Heritage Resource Assessment Report
- c) The East Fenwick Secondary Plan provides policies for the implementation of a community that is consistent with Provincial, Regional and local plans and policies. The community is planned to protect its natural heritage system, support the economic growth of Fenwick, and further enhance Fenwick's village character while establishing a pedestrian and cyclist friendly, compact and sustainable community. Appendices A and B are included as demonstration plans on how the policies of this document can be implemented; and,
- d) The East Fenwick Secondary Plan area shall be developed over time in conjunction with the applicable policies of the Official Plan of the Town of Pelham. Where there is a conflict between the policies of this Secondary Plan and the Official Plan, the policies of this Secondary Plan shall apply.

B1.9.2 Vision

The East Fenwick Vision is based on the growth and development tenets found in the Town of Pelham's Official Plan to deliver a new community that:

"Supports and emphasizes Fenwick's unique character, diversity, civic identity, rural lifestyle and heritage features and to do so in a way that is positive on the quality of life and health for the citizens who live and work in Pelham."

Furthermore, the Vision for the East Fenwick lands is intended to encourage and manage development in a manner that delivers a vibrant and diverse community. Key components of the Vision reflected in the preferred land use plan include:

1. The enhancement of Canboro Road as a scenic "promenade" route linking Fenwick to Ridgeville and Fonthill through the implementation of a distinctive streetscape and built form character strategy;
2. The preservation and enhancement of the existing streetscapes through the implementation of a special residential character zone that seeks to maintain the village's character through the implementation of Fenwick's typical streetscape and built form character elements such as building setbacks, height and massing along all existing roads;
3. The development of a diverse, neighbourhood-scaled open space network that is sensitive and complementary to the site's existing natural features;
4. The development of a community with a diverse range of housing types and home-based business opportunities; and,
5. The implementation of a connected street system.

B1.9.3 Neighbourhood Structure

- a) Schedule A7 identifies the planned neighbourhood structure. The neighbourhood structure is comprised of the following elements:
 - i. Two Residential Neighbourhoods – Each of these Neighbourhoods is anchored by open space at its centre and planned to contribute to the overall housing mix and density targets of the whole community as a whole. Policies permit the introduction of employment in the form of live-work units, subject to the development of appropriate zoning and the availability of home occupation opportunities as well as institutional land uses within each neighbourhood;
 - ii. The Greenlands System – The Greenlands System is comprised of existing natural heritage features, linkages, public parkettes, trails and stormwater management facilities. The system contributes to the overall structure of the Plan; and,
 - iii. The Street System – The Street System is comprised of an internal north south collector system intended to facilitate pedestrian, cycling and vehicular movement. The Street System extends into existing roads intersecting the study area, such as Sunset Drive, and delivers an internal street system that connects Welland Road to Memorial Drive.
- b) Schedule A11 demonstrates lands within the *Built Boundary* and the *Greenfield Overlay Areas*. The approach to the Secondary Plan has been to complement the Village of Fenwick's existing character and associated built and cultural heritage.

Per Policy B1.1.10.3 of the Town's Official Plan, the following density requirements shall be met:

- i. Lands within the Greenfield Overlay Area are subject to Town of Pelham minimum density of 20 units per hectare to meet the required 50 persons and jobs per hectare and
- ii. Lands within the Built Boundary Area are subject to a density range of 8 to 15 units per net hectare to contribute to the Town's overall intensification target.

B1.9.4 Plan Objectives

B1.9.4.1 General Development Objectives

These development objectives provide the framework for the planning and development of public and private lands within the Secondary Plan Area. The objectives will be implemented through the mechanisms set out in this Plan, including the implementing Zoning By-law and Site plan Approvals.

Introduction to Neighbourhood Structure (design objectives):

- a) To deliver a land use and community structure that supports the economic well-being of Fenwick's existing historic downtown area as defined by significant built heritage resources, archeological resources and cultural heritage landscapes;
- b) To ensure the community is developed in a compact sustainable manner that responds to the community's existing natural, cultural and servicing context;
- c) To develop a land use pattern that protects Fenwick's village character by ensuring that a land use transition is implemented along all existing roads;
- d) To create a sense of identity and community by ensuring the implementation of neighbourhoods that have a 2.5 to 5.0 minute walk to identifiable open space centres, based on an approximate 250 to 400 metres radius;
- e) To ensure there are residential housing options that promote lifecycle, and affordable housing opportunities, available to all existing and future residents;
- f) To conserve and enhance existing significant cultural heritage landscapes and built heritage resources as key character building elements to be integrated into the community's development;
- g) To ensure all new development occurs on the basis of full municipal infrastructure services, such as water and sewer, in addition to adequate utility networks;
- h) To phase development in a manner that is logical and cost effective without any undue financial burden to existing taxpayers and the surrounding farming community through the municipal review of servicing and transportation plans. The phasing of development within the Plan will be subject to the phasing and construction of municipal servicing infrastructure as identified in each particular development's required servicing and traffic reports, and any reports required through future Draft Plan of Subdivision processes.

Greenlands Structure objectives:

- a) To conserve and protect the area's natural heritage system as an integral component of the community by providing clear and unobstructed visual and physical links to the Secondary Plan natural features;
- b) To ensure a connected Greenlands system is implemented and comprised of the existing natural features, stormwater management facilities, streets, trails, and public parkettes;
- c) To encourage active transportation by delivering a network of connected parkland amenities with multi-use trails within walking distance to the majority of the surrounding neighbourhood residents;
- d) To accommodate for passive and small active recreational opportunities such as natural playgrounds as well as seating and walking areas as important community building components;
- e) To ensure stormwater management facilities are planned as essential Greenlands system components to be designed as special landscaped amenities that are publicly accessible;
- f) To encourage the integration and connectivity of existing natural features through the implementation of new linkages; and,
- g) To encourage the conservation and integration of existing hedgerow and woodland features into the community's overall design and development.

Street Structure objectives:

- a) To deliver complete streets in accordance to the Region's complete street guidelines with a clear road hierarchy based on a connected modified grid network that accommodates for all modes of travel fundamental in the implementation of an identifiable, great community;
- b) To deliver complete streets with a unique functional rural cross section character in the implementation of a community with a strong village character;
- c) To preserve Canboro Road and Memorial Drive cultural heritage character through the implementation of a special character or Village Residential zone and site specific road solutions based on these roads constrained right of way width; and,
- d) To facilitate the implementation of a future transit network by ensuring the community's minor collector road transverses throughout the entire East Fenwick area. The network will allow for a 2.5 to 5-minute walk to a transit stop and the implementation of a future transit loop throughout the entire Village.

B1.9.4.2 Sustainability & Healthy Communities

The community design elements structuring the East Fenwick Secondary Plan Area are the physical land use planning manifestation of a strategy that seeks to achieve social, economic, and environmental sustainability.

B1.9.4.2.1 Development in the East Fenwick Secondary Plan is envisioned to achieve:

1. Social sustainability by encouraging a diverse community that offers a diverse range and mix of housing including affordable, attainable and universally accessible options to meet housing needs at all stages of life;
2. Economic sustainability by continuing to encourage home occupation related employment and increasing the local population that can support Fenwick's downtown business community; and,
3. Environmental sustainability by focusing on maximizing support for climate action by lowering carbon emissions and delivering energy and water efficiency and circular systems.

B1.9.4.2.2 To achieve a sustainable and healthy community, the plan is envisioned to integrate key elements such as density, walkability, pedestrian and cycling routes, publicly accessible electric vehicle infrastructure, water efficiency infrastructure and landscaping and use of sustainable materials to maximize low carbon and low impact design.

B1.9.4.3 Affordable Housing

- a) It is the objective of this Plan that a minimum of 25% of all new residential development within the East Fenwick Secondary Plan Area meet the Provincial and Regional definition of affordable housing. The construction of affordable housing at initial development stages in an evenly distributed manner throughout the neighbourhood plan is highly encouraged. Affordable housing may be achieved by:
- i. Supporting the construction of an alternative range of housing to include stacked townhouses, back-to-back townhouses or apartment buildings;
 - ii. Encouraging larger size units in multiple forms to accommodate varying family sizes (i.e., 3-bedroom units);
 - iii. Supporting construction of additional residential units and encouraging building design that allows for additional residential units;
 - iv. Supporting sustainability initiatives, such as passive design or green technologies (e.g. low flow toilets), that keep operating costs of the homes or buildings lower;
 - v. Encouraging construction of condominium units with modest common element amenities to keep the housing and monthly condominium fees cost low; and,
 - vi. Encouraging the construction of rental units in a range of unit sizes and unit types to accommodate a diverse range of household sizes and incomes.
- b) The Town will work with other government agencies and the private sector to promote innovative housing forms, development techniques, and incentives that will facilitate the provision of affordable housing; and,
- c) Affordable housing is encouraged to be located in close proximity to local community facilities, future public transit routes and active transportation facilities.

B1.9.5 Design Policies

B1.9.5.1 Neighbourhood Demonstration and Active Transportation Plans and Accompanying Land Budget

- a) The Neighbourhood Demonstration and Active Transportation Plan and resulting land budget contained in appendices A and B illustrate one of the multiple variations in which the policies contained in this document can be implemented. The intent of the Neighbourhood Demonstration and Active Transportation Plan is to promote an appropriate built form, streetscape and public realm within the Secondary Plan Area.

B1.9.5.2 Urban Design Guidelines

- a) Urban Design Guidelines have been prepared to guide development (attached as Appendix C). The intent of the Urban Design Guidelines is to promote an appropriate built form, streetscape and public realm within the Secondary Plan Area. The Urban Design Guidelines function as an implementation tool for successive development and approval processes that are required to implement this Plan, particularly zoning, required Master Plans and Draft Plans of Subdivision/Condominium; and
- b) All development within the East Fenwick Secondary Plan Area shall be generally consistent with the Urban Design Guidelines attached to this Plan as Appendix C. Adjustments and further refinements to the Urban Design Guidelines are anticipated and shall not require an amendment to this Plan, provided that the intent and general design approach inherent to the Plan Vision is achieved to the satisfaction of Town staff.

B1.9.5.3 General Site Development Criteria

- a) The Town shall ensure that the following general site development criteria are implemented in all new development:
 - i. Buildings shall be street-oriented and provide direct street access for pedestrians;
 - ii. Surface parking areas shall generally be sited to the side or rear of buildings, or, preferably below grade. The softening of the impact of surface parking through building orientation and landscaping treatments is required;
 - iii. Residential driveway access and garage wall shall generally be set back from the living portion of the front façade or located at the rear of the building;
 - iv. All new apartment development shall provide secure indoor bicycle parking and outdoor bicycle racks at their main entrance. In locations with minimal setbacks where there is insufficient space for bicycle racks at the main building entrance, bicycle racks may be provided at an

- alternate location in proximity to the entrance that is user-convenient and highly visible;
- v. Compatibility between different land uses and scales of buildings shall be achieved through appropriate siting, building design, massing articulation and landscape treatment;
 - vi. High quality landscape treatment shall be provided throughout the Secondary Plan Area;
 - vii. Building form and siting shall minimize the impacts of noise, wind and shadows on adjacent properties;
 - viii. To ensure a gentler transition to Fenwick's rural character, higher density development shall generally be located towards the centre of the Secondary Plan Area adjacent to public open space areas to minimize the building massing and presence on the existing streetscape;
 - ix. Loading areas and outdoor fully enclosed refuse collection areas shall be unobtrusive and screened and shall be located at the side or the rear of buildings; and,
 - x. Rooftop equipment shall be unobtrusive, architecturally incorporated into the design of the building and/or screened from view.

B1.9.5.4 Crime Prevention Through Environmental Design (CPTED)

- a) The Town will promote building and site design that assists in the reduction of incidence of crime through the implementation of Crime Prevention Through Environmental Design (CPTED) principles including natural surveillance, natural access control, territorial reinforcement and space assessment; and,
- b) The Town shall have regard for the principles of CPTED in their review of all development applications.

B1.9.5.5 Sustainable/Green Building Design

- a) The overall development pattern within the Secondary Plan has been developed to encourage the following sustainable community design principles:
 - i. Street orientation to maximize passive solar gain and pedestrian comfort on streets;
 - ii. Shorter block lengths to encourage pedestrian permeability;
 - iii. Conserve, protect and integrate natural features within the Plan as an integral part of sustainably responding to the context by:
 - Conserving the natural heritage system;
 - Integrating hedgerows and appropriate woodlands into the community's open space and streets system;
 - iv. Street standards that encourage the retention of existing tree canopy with special consideration for the Plan's identified scenic roads of Canboro Road and Memorial Drive;

- v. Implement street standards that encourage a tree canopy and a green infrastructure approach to improve stormwater capture and reuse and maintain and enhance the area hydrological features and functions;
- vi. Support the choice of native planting materials and xeriscaping to address summer/winter conditions and reduce water consumption;
- vii. Support circular water and energy system opportunities to reduce water consumption;
- viii. Encourage the implementation of Electric Vehicle infrastructure in a manner that allows for future increased uptake throughout the neighbourhood at all public spaces;
- ix. Encourage a broadband network throughout the community that will use technology applications to support air quality and net zero neighbourhoods by efficient and low carbon transportation, signalization, work at home and energy and water efficiency opportunities;
- x. Support night sky lighting;
- xi. Encourage opportunities for community-based initiatives such as car-pooling, community composting, waste reduction, educational and stewardship programs; and,
- xii. Include renewable energy strategies and are designed to support net zero building techniques, restoring as much energy as they consume.

b) The Secondary Plan shall encourage the following green building design and sustainability requirements:

- i. New buildings achieve a 20% or greater water conservation efficiency than required by the Ontario Building Code;
- ii. New buildings, with the exception of residential buildings containing less than 25 dwelling units, to achieve the following sustainability requirements:
 - a. LEED Silver, or equivalent, prior to and including 2021; and
 - b. LEED Gold or equivalent from 2022 up and beyond 2031.
- iii. New development to be built to the following requirements for sustainability:
 - a. Grade related (3 storeys or less) residential buildings achieve a minimum performance level that is equal to ENERGY Star U standards; and,
 - b. Mid-rise (4 storeys and greater) residential buildings shall be designed to achieve greater energy efficiency than the Model National Energy Code for Building.

c) The Town shall encourage and support public and private partnerships and pilot projects for net zero water and energy systems that support climate action greenhouse gas reductions.

B1.9.6 Phasing Policies

- a) Development shall be phased to provide for continuous, orderly extension of the community to ensure the most efficient and economic use of the existing and proposed infrastructure. The following phasing criteria shall be considered in the review of all development applications:
- i. The development contributes to, or can be appropriately integrated within the logical sequence of construction of all required sewer, water, storm water, active transportation and other community facilities;
 - ii. The development satisfies all requirements regarding the provision of parkland and other facilities; and,
 - iii. The development implements any relevant findings of the studies required by the Official Plan and this Secondary Plan.

B.1.9.7 Land Use Designations

- a) Land use is divided into two Residential Neighbourhood and four Greenlands System designations and includes a special policy area overlay as shown in Schedule A10 with policies established for each category. The categories include:

The Residential Neighbourhoods designations:

- i. Village Residential
- ii. Neighbourhood Residential

The Greenlands System includes:

- i. Natural Heritage System
- ii. Special Policy Area
- iii. Environmental Linkages
- iv. Open Space / Parkette
- v. Stormwater Management Facilities

- b) The boundaries of land use designations, as shown on Schedule A10, are intended to be general and approximate, unless they coincide with an existing road, lot line, a prominent physical feature and/or relate to studies approved by Town, Regional, or Conservation Authority staff. Adjustments to the location of land use boundaries may be permitted without the need for an Official Plan Amendment provided the general intent of the Official Plan and this Secondary Plan are maintained and all required technical reports supporting the revision of land use boundaries are completed to the satisfaction of Town. Similarly, adjustments may be made in the location of streets, trails and bikeways provided the intent of the Secondary Plan is maintained to the satisfaction of the Town.

B1.9.8 Residential Uses

B1.9.8.1 Intent

It is the intent of the Town to promote the development of compete communities through an appropriate housing mix, opportunities for home occupation related employment and a pedestrian oriented street design. The two neighbourhoods shall be connected to the Greenlands System.

B1.9.8.2 General Policies

- a) In order to achieve the minimum density targets of this Plan, the Town will not approve any development application that compromises the ability for the Town to achieve the identified density targets throughout the East Fenwick Secondary Plan Area. As such, prior to the approval of any application for Plan of Subdivision and/or Zoning By-law Amendment, the proponent shall demonstrate how the proposed development contributes to the density targets of the Plan;
- b) Prior to the approval of any development application, proponents shall provide a housing mix and density plan that geographically distributes built form/ housing types, lot sizes and densities based on the objectives and policies of this Plan demonstrated through a plan of subdivision or site plan and planning justification report;
- c) Proposed development within the Secondary Plan Area shall generally conform to Appendix C – Urban Design Guidelines. If, in the opinion of the Town, the proposed street and block layout significantly departs from the design intent, the applicant shall be required to justify the proposed changes in the Planning Justification Report and accompanying explanatory Urban Design Brief, to be submitted at the time of submission of development application.
 - i. Urban Design Brief– The Urban Design Brief will provide a detailed description of the proposed new Draft Plan Vision to include road, block, land use, open space and streetscape components. Ultimately, the purpose of the Urban Design Brief is to demonstrate how the proposed Draft Plan implements the East Fenwick Secondary Plan Vision.
- d) Subject to the findings of the Environmental Impact Study required in Policy B1.9.9.3 to reduce or remove natural heritage features, any land use changes proposed for the Special Policy Area will require the applicant to undertake the studies/updates outlined in Policy B.1.9.8.2 e);
- e) Notwithstanding general compliance with Secondary Plan policies, technical supporting studies will be required as part of the Town's development proposal process for all development applications. Technical supporting studies, which can be based on the "Transportation Assessment" and "Municipal Servicing Conceptual

Design" reports accompanying this Secondary Plan, are to include, but are not limited to:

- i. Municipal Servicing Plan shall include, but shall not be limited to, technical details regarding the provision of water, wastewater, stormwater management and public/private utilities;
 - ii. Transportation Plan shall include, but shall not be limited to, technical details regarding the traffic conditions and implementation strategies proposed to accommodate for development;
 - iii. Environmental Impact Study to include the assessment of lands within and adjacent the Natural Heritage System and Special Policy Area Overlay identified in this Plan (refer to Town Official Plan policies B3.2.4.3 and B3.3.4.3).
- f) Within the Neighbourhood Residential designation, increases in density may be permitted to the satisfaction of the Town without Amendment to this Plan, subject to:
- i. A test of functional servicing compatibility addressing all water, wastewater, stormwater through an update to the Municipal Servicing Plan;
 - ii. A Traffic Impact Study demonstrating traffic impacts and requirements; and,
 - iii. An Urban Design Brief to demonstrate how the proposal implements this Plan's Urban Design Guidelines and overall Vision intent.

B1.9.8.3 Village Residential

B1.9.8.3.1 Permitted Uses

a) The following uses are permitted within the Village Residential designation:

- i. Single detached and semi-detached residential buildings;
- ii. Accessory apartments/secondary units subject to Policy B1.1.4;
- iii. Accessory buildings and structures related to the primary residential dwelling unit;
- iv. Home occupations subject to Policy B1.1.6;
- v. Places of worship;
- vi. Residential care facilities subject to Policy B1.1.8.1;
- vii. Day nurseries;
- viii. Open space linkages;
- ix. Community meeting spaces;
- x. Public uses; and,
- xi. Public and private utilities.

b) In addition to the permitted uses identified in Policy B1.9.8.3.1 above, street and block townhouses may be permitted provided garage areas are not visible from the public right of way in accordance with this Plan and accompanying Urban Design Guidelines.

B1.8.9.3.2 Development Policies

- a) For single detached lots density shall range from a minimum of 8 units per net hectare, up to 20 units per net hectare;
- b) For semi-detached lots density shall range from a minimum of 20 units per net hectare, up to 40 units per net hectare;
- c) Permitted street townhouse dwellings shall be developed at densities ranging from a minimum of 20 units per net hectare, up to 50 units per net hectare;
- d) Built form siting along the Plan's perimeter and Canboro Road should be in keeping with existing setback parameters;
- e) Residential buildings along Canboro Road shall minimise their garage and driveway presence onto the streetscape as outlined in the Urban Design Guidelines;
- f) The maximum residential building height for any building within the Village Residential designation shall be 2.5 storeys; and,
- g) Places of worship and day nurseries shall only be permitted within the Village Residential designation through a Site-Specific Zoning By-law Amendment process provided that they:
- h)
 - i. Have a minimum lot size of 0.75 of a hectare;
 - ii. Are compatible with adjacent properties in terms of aesthetics, privacy, sun shadowing, noise and/or other impacts; and,
 - iii. The landmark or steeple component of a Place of Worship may be permitted to exceed the 9 metres height limitation, subject to approval by the Town, through the required Site-Specific Zoning By-law Amendment process.

B1.9.8.4 Neighbourhood Residential

B1.9.8.4.1 Permitted Uses

- a) The following uses are permitted within the Neighbourhood Residential designation:
 - i. Single detached and semi-detached residential buildings;
 - ii. All forms of townhouse and multiple dwelling residential buildings;
 - iii. 4 to 6 storey apartment buildings;
 - iv. Accessory apartments/secondary units subject to Policy B1.1.4;
 - v. Live-work units;
 - vi. Residential care facilities subject to Policy B1.1.8.1;

- vii. Accessory buildings and structures related to the primary residential dwelling unit;
- viii. Home occupations subject to Policy B1.1.6;
- ix. Places of worship;
- x. Day nurseries;
- xi. Parks, parkettes and open space linkages;
- xii. Community Amenity buildings;
- xiii. Public uses; and,
- xiv. Public and private utilities.

B1.9.8.4.2 Development Policies

- a) For single detached lots density shall range from a minimum of 18 units per net hectare, up to 30 units per net hectare;
- b) For semi detached lots of density shall range from a minimum of 20 units per net hectare, up to 50 units per net hectare;
- c) Permitted street and block townhouse as well as multiple dwelling residential buildings shall be developed at densities ranging from a minimum of 20 units per net hectare, up to 60 units per net hectare;
- d) Small scale apartment buildings shall be developed at densities ranging from a minimum of 73 units per net hectare, up to 120 units per net hectare;
- e) The maximum building height for any building within the Neighbourhood Residential designation located internal to the community shall be 6 storeys and may include a 4.5 metre minimum ceiling height requirement on the ground floor to accommodate live-work units;
- f) The maximum building height for any building within the Neighbourhood Residential designation with frontage onto Canboro Road, Welland Road and/or Cream Street shall be 4 storeys;
- g) Places of worship and day nurseries shall only be permitted within the Neighbourhood Residential designation through a Site-Specific Zoning By-law Amendment process provided that they:
 - i. Have a minimum lot size of 0.75 of a hectare;
 - ii. Are compatible with adjacent properties in terms of aesthetics, privacy, sun shadowing, noise and/or other impacts;
 - iii. Are located near the intersection of two public roads, at least one of which is a Collector Road; and,
 - iv. The landmark or steeple component of a Place of Worship may be permitted to exceed established height limitations, subject to approval by

the Town, through the required Site Specific Zoning By-law Amendment process.

- h) Live work uses shall only be permitted within the Neighbourhood Residential designation through a Site-Specific Zoning By-law Amendment process provided that they:
- i. Are limited in size to 150 square metres per live work unit
 - ii. Are compatible with adjacent properties in terms of aesthetics, privacy, noise and/or other impacts;
 - iii. Are located near the intersection of two public roads; and,
 - iv. Are located near or at the centre of the neighbourhood, within a 2.5-minute walk from the majority of residents.
- i) Development on the basis of garage and driveway access from public or private lanes is encouraged, and may be required where dwelling units front onto a Minor Collector Road, a Linkage, or a linear park. Development on the basis of garage and driveway access from public or private lanes may be permitted where dwellings front onto a Local Road, subject to confirmation of the technical requirements by the Town;
- j) Neighbourhood Residential development that abuts lands designated Village Residential shall be designed to:
- i. Ensure compatibility to Village Residential areas through deeper lots;
 - ii. Ensure compatibility to Village Residential areas through the conservation, when present, of the existing tree canopy in the form of a landscaped buffer, which shall be a minimum of 10 metres in depth;
 - iii. Increased setbacks;
 - iv. The provision of landscaped buffers when backing onto Village Residential areas;
 - v. The provision of clearly demarcated vehicular and pedestrian access points; and,
 - vi. The provision of screened parking areas located behind the principal building with limited exposure to adjacent public roads.
- k) Schedule A10 identifies symbolically (with the use of an asterix) the potential location for multiple dwelling residential buildings or apartment buildings. These locations have been chosen due to their landmark, gateway role as well as their proximity to large open space areas. It is important that future development on these sites facilitates strong view terminuses across the Plan Area, accentuates the Greenlands system rather than detracts from it, and maximizes visual prominence and anchoring of the buildings along street axes. To achieve this, in general proximity to that symbol, a multiple dwelling residential building or an apartment building may be developed subject to appropriate zoning and the following criteria:
- i. The site is located along public roads;

- ii. The site does not exceed 10,000 square metres in size;
 - iii. Any proposed building at the corner of Cream Street and Canboro Road shall be designed as a priority lot that fits with Fenwick's Village character; and,
 - iv. Apartment buildings located within the Residential Neighbourhood lands that are more than 4 storeys in height shall be designed following the angular plane strategy of this Plan's Urban Design Guidelines of Appendix C.
- l) Apartment buildings shall be designed to ensure development compatibility to surrounding existing and future built forms. Site design considerations may include:
- i. Increased setbacks;
 - ii. Sensitive building siting;
 - iii. Provision of landscape buffers;
 - iv. Angular planes or other mechanisms that control the scale of development;
 - v. The requirement for structured parking and/or the provision of parking areas that do not dominate the site physically and visually from the public right of way;
 - vi. Bicycle parking is provided in areas that are located near a building access, are protected from the weather, are safe and well illuminated, and do not obstruct the public sidewalk;
 - vii. Bicycle parking is provided at a minimum of 1 space per unit plus 0.5 visitor/short term spaces per dwelling unit;
 - viii. The location, pattern and style of entranceways, windows, balconies and other architectural details ensure matters of character fit, oversight and privacy are addressed; and,
 - ix. The location, pattern and style of loading bays and refuse containers ensures these areas internal to the site, and are not visible from the public right of way.

B1.9.9 The Greenlands System

The East Fenwick network of green land spaces is made up of the area's existing Natural Heritage System, a series of interconnected small neighbourhood parkettes, environmental linkages, active transportation network and trails, and stormwater management areas. The proposed open space network encourages active transportation, recreation, and opportunities to connect to the natural heritage system and the neighbourhood's open space amenities as an integral part of the neighbourhood design. The Greenlands System recognizes natural heritage features and their associated functions and seeks to maintain connections between and among them, so that their existing ecological and hydrological functions are maintained or enhanced, where possible.

Note: The East Fenwick Secondary Plan, specifically its environmental mapping, reflects significant investment from landowners in Environmental Impact Studies (EIS's) that cover portions but not all of the Plan Area. Areas where existing studies have been conducted reflect the regulatory standards of the approvals framework that existed at time of their completion. Because of this, more explicit policy direction is provided for these portions of the Plan Area relative to others. It is important to note that policy provisions included in this Secondary Plan reflect the regulatory standards in place at time of application, and future development may be held to different standards of environmental review as these regulations evolve over time.

B1.9.9.1 Intent

- a) It is the intent of the Town to establish a linked Green Lands System that also acts as a fundamental community structure element for the East Fenwick Secondary Plan;
- b) The Green Lands System, as identified on Schedule A8, is comprised of the following components and associated buffers:
 - i. Natural Heritage System
 - ii. Special Policy Area Overlay
 - iii. Environmental Linkages
 - iv. Stormwater Management
 - v. Open Space/Parkette
- c) The inclusion of all the associated elements and related activities into an interconnected Greenlands System will protect and enhance the natural heritage system and its associated functions and expand the recreational opportunities available to residents;
- d) Linkages through the implementation of corridors provide important connections between natural heritage features. Linkages and corridor connections shall be reviewed as part of an EIS. Where identified through an EIS or other natural heritage study or plan, linkages and corridors shall be incorporated into development plans and protected through use of an appropriate zone in the Town's Zoning By law.
- e) As part of a future development application, additional scoped EIS work is required to address environmental restoration matters relating to the overall area, location and composition of the restoration required to accommodate the proposed storm water management facility located at the south-west corner of Welland Road and Cream Street.

B1.9.9.2 Natural Heritage System

- a) The Natural Heritage System designation recognizes the limits and associated minimum buffers of the Provincially Significant Wetland.
- b) The Natural Heritage System designation recognizes environmental features defined through an EIS.
- c) The ecological and hydrologic functions of the lands designated Natural Heritage System shall be protected, maintained, or, where possible, improved for the long-term, recognizing linkages between and among natural heritage features and areas. The Natural Heritage System designation is intended to:
 - i. Protect the health and water quality of the natural heritage feature;
 - ii. Conserve biodiversity;
 - iii. Protect significant natural heritage features and their associated functions; and
 - iv. Protect surface and ground water resources.
- d) The lands designated Natural Heritage System on Schedule A8 are also subject to Section B3.2 of the Official Plan.

B1.9.9.3 Special Policy Area Overlay

- a) The Special Policy Area Overlay recognizes natural heritage features, which contribute to the Natural Heritage System.
- b) Additional environmental study is required to determine the exact boundaries and buffers to protect the features and functions to the satisfaction of the Town and Region.
- c) The proponent shall be required to submit an EIS as part of any future development application to demonstrate that, over the long term, there will be no significant negative impact on the natural heritage features or their functions. The scope and content of the EIS shall be determined through the development of a Terms of Reference in consultation with the Town, Region and the NPCA.

- d) Should it be demonstrated that a natural heritage feature may be reduced or removed, the proponent shall incorporate portions of the feature into development plans, where feasible, to the satisfaction of the Town.
- e) Should the feature be reduced in size, the adjacent land use designation shall apply.
- f) The lands with the Special Policy Area Overlay on Schedule A8 are also subject to Section B3.3 of the Official Plan.

B1.9.9.4 Environmental Linkages

- a) The Environmental Linkages designation recognizes the importance of providing ecological corridors connecting identified Greenlands areas throughout the Secondary Plan Area.
- b) Environmental Linkages shall be established consistent with Section 3.1.17 of the Niagara Region Official Plan.
- c) Notwithstanding Section 3.1.17 of the Niagara Region Official Plan (Linkages), these areas shall be landscaped with native plantings to the satisfaction of the Town in a way that reinforces an ecological corridor consistent with the significant features it is connecting.
- d) Environmental Linkages shall integrate LID measures where possible to contribute to the overall storm water management strategy.
- e) Environmental Linkages will be dedicated to the Town at time of subdivision.

B1.9.9.5 Open Space / Parkette

- a) The policies applicable to the lands designated Open Space / Parkette on Schedule A8 are found in Section D5.4 of the Official Plan. In addition to the policies of Section D5.4, it will be required that Parkettes are located at the centre of each neighbourhood as identified in Schedule A7, a minimum of two (2) parkettes shall be provided across the plan area to ensure easy access and multiple opportunities for rest, relaxation and access to nature;
- b) Amendments to this Plan's Greenlands structure and resulting trail system / active transportation network shall be completed to the satisfaction of the Town in consultation with the Pelham Active Transportation Committee; and
- c) The following policies apply to the establishment of parkettes:
 - i. A parkette shall generally have a minimum frontage on the abutting sidewalk of 20 metres, and a minimum depth of 30 metres with a

- minimum area of 0.4 of a hectare and shall be generally consistent with the Urban Design Guidelines attached to this Plan as Appendix C;
- ii. Parkettes shall be designed to reinforce a high-quality relationship between the public right of way and surrounding land uses as a component of the Greenlands System; and,
 - iii. Parkettes shall incorporate/include existing vegetative features such as hedgerows and non-significant woodlots as part of their design; and,
 - iv. Landscape elements and features within the parkettes shall be designed to define and articulate activity areas, circulation, entry points, seating and gathering areas.
- d) Parkettes that are less than 0.4 of a hectare shall generally not contribute to the Town's parkland dedication requirements and shall generally not be required or accepted by the Town. The Town may, however, consider accepting parkland parcels that are less than 0.4 of a hectare without Amendment to this Secondary Plan, subject to the specific approval by Council that is based on an understanding of the specific land parcel's contribution to the overall public parkland system within the East Fenwick Secondary Plan Area.

B1.9.9.5.1 Parkland Conveyance Reduction

- a) Parkland conveyance requirements may be reduced under the following circumstances, subject to approval by Council:
- i. When land is developed for long term care facility use, as defined by the Province, a maximum of 2% of the total land area shall be conveyed to the Town;
 - ii. When the proposed development is being undertaken by a public-sector organization the parkland conveyance requirement may be eliminated or reduced at the discretion of Council;
 - iii. When land is developed to include affordable housing units, as defined by the Province, the conveyance requirement may be eliminated or reduced proportionate to the number of affordable units provided, at the discretion of Council;
 - iv. When the proposed development is being undertaken by a not-for-profit organization, as defined by the Province, the parkland conveyance requirement may be eliminated or reduced at the discretion of Council; and/or,
 - v. When a development proposal includes a property designated or listed under Part IV or V of the Ontario Heritage Act or listed on a Municipal Heritage Register under Section 27 of the Ontario Heritage Act, or has otherwise been recognized as a significant cultural heritage landscape or built heritage resource, and the proposal incorporates and conserves a cultural heritage resource to Town satisfaction, a parkland conveyance reduction of 50% of the applicable conveyance requirement for that specific property may be applied.

B1.9.9.6 Stormwater Management Facilities

- a) The policies related to stormwater management facilities within the East Fenwick Secondary Plan Area are found in Section C6 of the Official Plan. Their general location is conceptually shown in Schedule A8;
- b) General objectives for stormwater management facilities within this Secondary Plan Area are:
 - i. To maintain, and where possible, improve the health and condition of the receiving/adjacent watercourses;
 - ii. The achievement of no net increase in stormwater run-off from the Secondary Plan Area to adjoining lands;
 - iii. To maintain, and where possible, improve the quality of stormwater entering surface and groundwater supplies; and,
 - iv. To promote the use of naturalized methods of stormwater management.
- c) In addition to the policies of Section C6 of the Official Plan, the following policies apply within the East Fenwick Secondary Plan Area:
 - i. The Plan's stormwater management facilities shall be permitted in all land use designations within the East Fenwick Secondary Plan Area, except for Natural Heritage System lands, their general location shown in Schedule A8;
 - ii. Notwithstanding B1.9.9.6 c) i), the stormwater management facility located south of Welland Road at Cream Street is permitted within the Natural Heritage System provided the facility is designed to the specifications agreed upon by the Niagara Region and Niagara Peninsula Conservation Authority;
 - iii. Stormwater management facilities may be permitted in Special Policy Area overlays without the need for a further Amendment to this Plan, subject to the policy direction provided in this Plan, including the preparation of an Environmental Impact Study (EIS) to the satisfaction of the Region in consultation with the Town and the NPCA showing no negative impact to feature or function;
 - iv. Stormwater management facilities will be designed as essential components within the open space network and be publicly accessible where appropriate;
 - v. Stormwater management facilities will blend with the natural landscape, organic shapes and landform grading designed to replicate natural landforms in the area while being safe to visitors. Inlet and outlet structures will be concealed using a combination of planting, grading and natural stone;
 - vi. Stormwater management facilities will not be fenced, but rather will be designed with trails and interpretative signage;
 - vii. Stormwater management facilities shall be provided with high quality landscaping designed by a qualified Landscape Architect; and,

- viii. Stormwater management facilities shall be designed generally consistent with the Urban Design Guidelines attached to this Plan as Appendix C.

B1.9.9.6.1 Stormwater Management Design Criteria

- a) In order to avoid the negative impacts of flooding in the downstream watershed, all new development post development peak stormwater flows shall be attenuated to pre-development conditions for up to and including the 100 year return period design storm event.
- b) In order to avoid negative impacts of stormwater sediments, stormwater from all new development should be managed for quality purposes to an enhanced standard (i.e. 80 percent long-term suspended solids removal).
- c) Adequate and safe minor and major storm conveyance systems shall be required for all new development.

B1.9.9.6.2 Low Impact Development Design

- a) With the goal of replicating the existing hydrologic regime, the Town shall encourage the implementation of low impact design standards as outlined in the Ministry of Environment, Conservation and Parks "Low Impact Design Stormwater Management Guidance Manual, which emphasize the use of bio-swales, innovative stormwater practices, and at-source infiltration, in order to contain stormwater runoff from the 90th percentile storm (25mm) on site;
- b) With the goal of minimizing the negative impacts of urbanization on the area's natural heritage features, the Town shall encourage low impact design methods when treating stormwater runoff to treatment train to an enhanced standard (the highest standard as prescribed by the Ministry of the Environment, Conservation, and Parks);
- c) With the goal of adapting to climate change and mitigating the impacts of severe storm events, the Town shall encourage the following;
 - i. Include robust major overland flow routes through all new development to reduce the risk of overland flooding.
 - ii. New development should be designed to minimize the risk of basement flooding from groundwater infiltration.
 - iii. Heating, ventilation, and air conditioning (HVAC) and electrical systems should be well elevated from the basement floor or located above grade.
 - iv. If the home foundation drain connects to a storm sewer, a backwater valve should be installed on the storm sewer lateral to prevent stormwater from backing up into the basement if the storm sewer is overloaded.
 - v. If the home foundation does not connect to a storm sewer, a primary and back-up sump pump should be installed complete with a back-up power system.

- vi. Driveways should be built to slope away from homes or garages (i.e. reverse slope driveways should not be permitted).
 - vii. Consideration should be given to increasing the capacity of all new storm infrastructure by a minimum of 25% to account for more frequent and severe rainfall events.
 - viii. Consideration should be given to implementing a minimum building setback from all mapped floodplains to account for more severe rainfall events.
- d) The Town shall encourage the introduction of green infrastructure, such as bioswales, within the street rights-of-way to enhance ground water infiltration and improve water quality as part of a comprehensive water management plan;
- e) The Town shall encourage a rainwater harvesting program to provide the passive irrigation of public and/or private greenspace, including absorbent landscaping, landscaped walls, cisterns, rain barrels, underground storage tanks and/or infiltration trenches; and,
- f) The Town shall encourage xeriscaping using native, drought-tolerant plants, a cost-effective landscape method to conserve water and other resources on a community-wide level.

B1.9.10 Environment and Groundwater Management Policies

- a) The policies related to the environment and groundwater management within the East Fenwick Secondary Plan Area are found in Section C5 of the Official Plan and
- b) In addition to Section C5 of the Official Plan, development applications may need to be supported by a Hydrogeological or Hydrological Report as determined by the Town and Region at the time of pre-consultation. The purpose of the report is to investigate the impacts of the proposed development on water quality and quantity and provide recommendations on:
- i. How to maintain or enhance the quality and quantity of the water resource;
 - ii. How to minimize or eliminate the effect of the proposed use on the groundwater recharge function and maintain water balance;
 - iii. How to minimize or eliminate the effect of the proposed use on the quality and quantity of drinking water in adjacent private and municipal wells;
 - iv. How to maintain or enhance sensitive groundwater recharge/discharge areas, aquifers and headwater areas;
 - v. Whether it is required to monitor water budgets for groundwater aquifers and surface water features; and;

- vi. How to ensure that the quality of the watercourses affected by the development are maintained; and,
- vii. How to control or manage the risk of flooding or erosion.

B1.9.11 General Development and Infrastructure Policies

B1.9.11.1 Water and Sewer Servicing Strategy

- a) The policies related to water and sewer servicing within the East Fenwick Secondary Plan Area are found in Section D1.1 of the Official Plan;
- b) Notwithstanding Section D1.1 of the Official Plan, water and sewer servicing within the East Fenwick Secondary Plan Area shall be as follows:
 - i. A Master Servicing Plan, outlining preliminary functional water distribution, wastewater collection and stormwater management facilities, including designs and costs, will be prepared prior to the approval of any development within the Secondary Plan Area;
 - ii. All required infrastructure shall be constructed and sized to accommodate the full build-out of the Secondary Plan Area. As a result, some of the infrastructure required by one proponent may be oversized to service the larger area. Benefiting landowners shall be required to compensate provider proponents, prior to development on the benefiting landowner's property through a cost sharing agreement;
 - iii. All development within the Secondary Plan Area will proceed according to individual, site specific Drainage Plans and Servicing Plans based on the proposed Draft Plan of Subdivision and all other accompanying requirements set by this Plan by the proponent, to the satisfaction of the Town and the Region; and,
 - iv. Development may be phased to coincide with the availability of servicing and financial capabilities necessary in the delivery of all required municipal services.
- c) Notwithstanding the policies contained in B1.9.9, a pump station is permitted to be located within the Greenlands designation south of Welland Road at Cream Street.

B.1.9.11.2 Transportation

- a) The policies related to transportation within the East Fenwick Secondary Plan Area are found in Section D2 of the Official Plan;
- b) Notwithstanding Section D2.3 of the Official Plan, rights of way for new public roads within the East Fenwick Secondary Plan Area shall be as follows:
 - i. For Minor Collector Roads, the right of way widths shall be between 22.0 to 34.0 metres;

- ii. For Local Roads, the right of way width shall be 16.0 to 20 metres; and,
 - iii. For Public or Private Laneways, the right of way width shall be 7.5 and 9.0 metres.
- c) Notwithstanding Section D2.3 of the Official Plan, right of ways for existing public roads within and bounding the East Fenwick Secondary Plan Area shall be as follows:
- i. For Memorial Drive, the right of way width shall be between 20.0 and 22.0 metres;
 - ii. For Canboro Road, the right of way width shall be between 20.0 and 24.0 metres;
 - iii. For Welland Road, the right of way width shall be between 20.0 and 24.0 metres;
 - iv. For Balfour Street, the right of way width shall be between 20.0 and 22.0 metres; and,
 - v. For Cream Street, the right of way width shall be between 20.0 and 22.0 metres.
- d) All roads within the East Fenwick Secondary Plan Area shall be developed to be consistent with the design intent illustrated in the Urban Design Guidelines attached to this Plan as Appendix C;
- e) Mid-block connectors should be strategically located throughout future developments to enhance active modes access to the Greenlands system;
- f) Mid-block connectors should be a minimum of 6.0 metres in width;
- g) In addition to those policies, the following policies apply to the trail system within the East Fenwick Secondary Plan area:
- i. A comprehensive trail system is conceptually identified on Schedule A8 and Appendices A and C. It is the intent of the Town to achieve this trail network through the appropriate processes required to facilitate the development of this Plan including an Environmental Impact Study process;
 - ii. The conceptual trail system includes trails within the natural heritage system (pending the outcome of an EIS), stormwater management facilities, linkages, open spaces and the road system;
 - iii. Adjustments and alternatives to the trails network identified on Schedule 8 shall be permitted subject to the Town being satisfied that the principle of connectivity and continuity of the system has been fulfilled;
 - iv. Trail design and type will be based on each site's sensitivity in order to mitigate impacts to the Natural Heritage System; and,

- v. Trails for pedestrians and cyclists within the East Fenwick Secondary Plan Area shall be developed generally consistent with the Urban Design Guidelines attached to this Plan as Appendix C.
- h) Protection of the site's existing hedgerows and non-significant woodland areas shall be implemented through the conservation and integration of these features as an integral part of the community open space structure, generally visible from or as part of the public right of way or as rear lot conditions. The protection of the site's existing hedgerows and non-significant woodland areas shall also implement Low Impact Design features outlined Policy B1.9.9.6.1, where feasible; and,
- i) The Canboro Promenade, shown on Schedule A8/9, shall be implemented as private development extends east along Canboro Road. The objective of the Canboro Promenade is to implement a safe and comfortable pedestrian and cycling link along the north side of Canboro Road where the majority of new private development is expected to take place. The implementation and coordination of the Promenade will be undertaken as part of the Draft Plan of Subdivision process to ensure the required right-of-way is secured through this process and shall be designed in accordance with the direction contained in Appendix C – Urban Design Guidelines.

B1.9.11.3 Utilities

- a) Ensure utility services such as hydroelectric power, communications/telecommunications, pipelines and natural gas lines are readily available to all development at levels necessary to ensure the safety and convenience of existing and future residents;
- b) The Town shall promote utilities to be planned for and installed in initial common trenches, below ground where feasible, in a coordinated and integrated basis in order to be more efficient, cost effective and minimize disruption;
- c) Utility services shall be developed to be compatible with the general character of the surrounding uses and minimize visual impact, where feasible. The Town will encourage utility providers to consider innovative methods for containing utility services on or within streetscape features such as gateways, lampposts, etc. when determining appropriate location for large utility equipment and utility cluster sites; and,
- d) Utilities location and screening within the East Fenwick Secondary Plan Area shall be developed generally consistent with the Urban Design Guidelines attached to this Plan as Appendix C.

B1.9.11.4 Heritage and Archaeological Resources

- a) The policies related to the heritage and archaeological resources within the East Fenwick Secondary Plan Area are found in Section D4 of the Official Plan; and
- b) Notwithstanding Section D4 of the Official Plan, built heritage and cultural heritage landscapes within the East Fenwick Secondary Plan Area shall be conserved as follows:
 - i. Development applications should conserve significant built heritage and cultural heritage resources in order to foster a sense of place and benefit communities, including the First Nations and Metis communities, consistent with the Heritage resources identified in Appendix E.
 - ii. Development shall be planned to conserve Canboro Road and Memorial Drive as significant cultural heritage landscapes.
 - iii. Development shall be planned to conserve significant built heritage resources and cultural heritage landscapes located at 1159 Cream Street, 655 Canboro Road, 645 Canboro Road, 1128 Cream Street and 646 Welland Street. Where impacts are expected to these properties, a Heritage Impact Assessment should be prepared in accordance with policies found in Section D4 of the Official Plan.
 - iv. Where impacts are expected to properties located at 704 Canboro Road, 695 Canboro Road, 606 Canboro Road, 607 Canboro Road, 615 Canboro Road, 668 Canboro Road, 675 Canboro Road, 687 Canboro Road, 688 Canboro Road, 691 Canboro Road, 1108 Cream Street, 1118 Cream Street, 1162 Cream Street, 578 Welland Road, 630 Welland Road, 663 Welland Road, a heritage impact assessment should be prepared in accordance with Section D4 of the Official Plan.
 - v. Development adjacent to identified built heritage resources and cultural heritage landscapes shall require a Heritage Impact Assessment prepared in accordance with Section D4 of the Official Plan.
 - vi. Designation of properties of cultural heritage value under the *Ontario Heritage Act* is encouraged.
 - vii. Archaeological Assessments shall be required on lands containing archaeological resources or areas of archaeological potential, where development and site alteration is proposed. Development and/or site alteration shall not be permitted unless significant archaeological resources have been conserved or the land has been investigated and cleared or mitigated following clearance from the Province. First Nations shall be consulted by the proponent at the earliest opportunity.

B1.9.11.5 Subdivision of Land

- a) The policies related to the subdivision of land within the East Fenwick Secondary Plan Area are found in Section D5 of the Official Plan;
- b) In addition to the policies of Section D5 of the Official Plan, the following policy applies to all lands within the East Fenwick Secondary Plan Area:

- i. It is the intent of the Town to ensure that Draft Plans of Subdivision are developed within a reasonable time frame. As such, the following statement shall be added to all Conditions of Draft Plan Approval:
 - A Draft Plan Approval extension may be extended pursuant to Subsection 51(33) or 51(32) of the *Planning Act*, but no extension can be granted once, or if, the approval has lapsed. If the owner wishes to request an extension to Draft Plan Approval, a written explanation must, together with a resolution from Council, be received by the Town of Pelham sixty days prior to the lapsing date; and a Draft Plan Approval extension will only be granted if, in the opinion of the Town, significant progress toward final approval has been achieved. Dormant Draft Plans shall not be considered for an extension of Draft Plan Approval.
- c) In addition to the policies of Section D5 of the Official Plan, the following policy applies to all lands within the East Fenwick Secondary Plan area:
 - i. For each of the two Residential Neighbourhoods, the Town encourages the benefitting landowners to work together, and to enter into a Developer's Group Agreement based on the requirements of this Plan;
 - ii. It is intended that there be at least one Developer's Group Agreement for the Residential Neighbourhood north of Canboro Road and one for the Residential Neighbourhood between Canboro and Welland Roads;
 - iii. It is recognized that in circumstances where there are multiple landowners, a singular Developer's Group Agreement may not be possible. To be considered a Developer's Group Agreement in fulfillment of the requirements of this plan, the Agreement must include a landowner or landowners that represent at least 50% of the redevelopment landholdings total area within the subject Neighbourhood;
 - iv. If there is a landowner that represents at least 50% of the landholdings, the Town may enter into a Development Agreement with the landowner;
 - v. Where undue delays are encountered with respect to execution of the required Developer's Group Agreements, the Town may, upon the request of an applicant landowner, attempt to resolve such difficulties or delays. Where resolution of such difficulties is deemed not possible by the Town, despite the Town's intervention, the Town shall approve alternative mechanisms to satisfy the intent of the applicable policies of this Plan;
 - vi. If a benefitting landowner within a Residential Neighbourhood wishes to submit a development application and has been unable to obtain the necessary participation of other benefitting landowners through a Developer's Group Agreement, notwithstanding best efforts to do so, then:
 - The applicant landowner shall notify the Town in writing that it proposes to submit an application for development on its lands, or a portion thereof, without a Developer's Group Agreement;
 - The applicant landowner in said notice shall set out, in compliance with the provisions of this Plan that the development proposal can be

accommodated on its lands, without the participation of other landowners or lands, appropriate urban development that implements the Neighbourhood Structure and policy intent of this Plan. The applicant landowner shall also elect in said notice, at its option, either to:

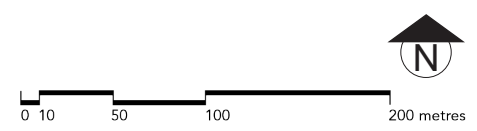
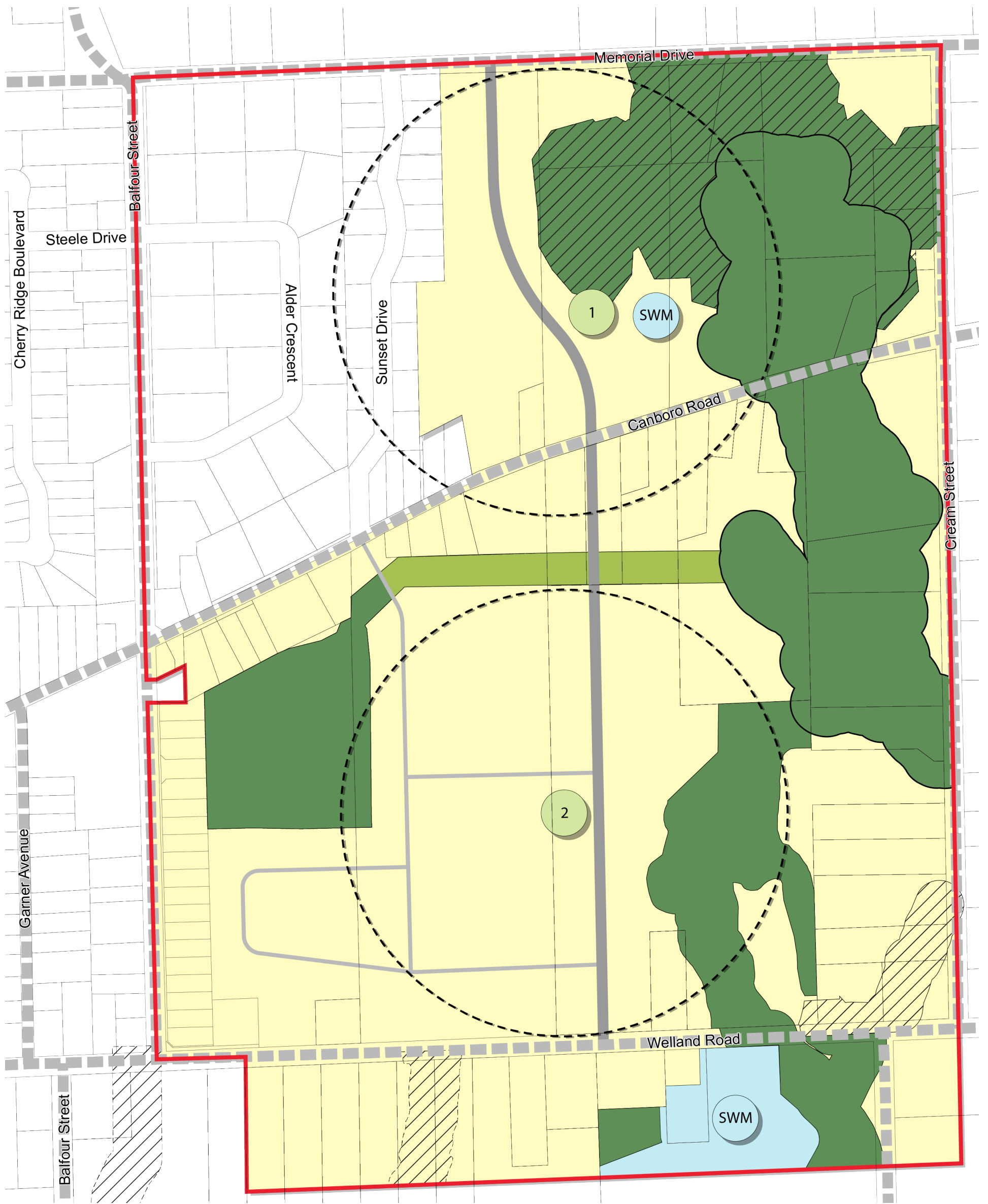
- Enter into a Development Agreement with the Town in lieu of other benefitting landowners; or,
 - Enter into a Front-Ending Agreement with the Town in accordance with the provisions of Section 44 of the Development Charges Act, as amended.
 - The purpose of these alternative agreements shall be to ensure that a non participating, benefitting landowner cannot develop their lands without appropriate compensation to the applicant landowner for any expenditures that exceeds the amount required to develop the applicant landowner's lands, and is a direct benefit to a non-participating, benefitting landowner; and,
 - Nothing in this policy compels the Town to enter into any agreements with any applicant landowner.
- vii. The Town, as appropriate, shall require the use of local Area-Specific Development Charge By-Laws or Front-Ending Agreements under the Development Charges Act, or other suitable arrangements among landowners, in order to implement the development of the Secondary Plan Area and to fairly allocate the costs of development. The Town shall ensure that stormwater facilities are included in any local Area-Specific Development Charges. Such agreements are intended to ensure that the necessary approvals and the required contributions of funds, lands and commitments for services will be in place and operative prior to, or coincide with the development and use of land. The Developer's Group Agreement and/or Front-Ending Agreement may deal with:
- Front-end or accelerated payment requirements;
 - Local services as permitted in Sections 44(1) and 59(2) of the Development Charges Act;
 - Matters to which the parties voluntarily agree; and/or,
 - Other matters permitted by law.
- viii. The Town must ascertain and be assured that a Developer's Group Agreement assigns cost sharing and other responsibilities in a reasonable and equitable manner, and, generally speaking, will do so in direct proportion that a development benefit is conferred upon the lands being developed. Subject to the appeal mechanisms identified in the *Planning Act* or the Development Charges Act, the issuance of final development approvals or the release of land for development, shall be subject to the execution of a Developer's Group Agreement and/or Front-Ending Agreement.

B1.9.12 Plan Implementation and Administration

- a) The policies applicable to plan implementation and administration within the East Fenwick Secondary Plan Area are found in Section E1 of the Official Plan.

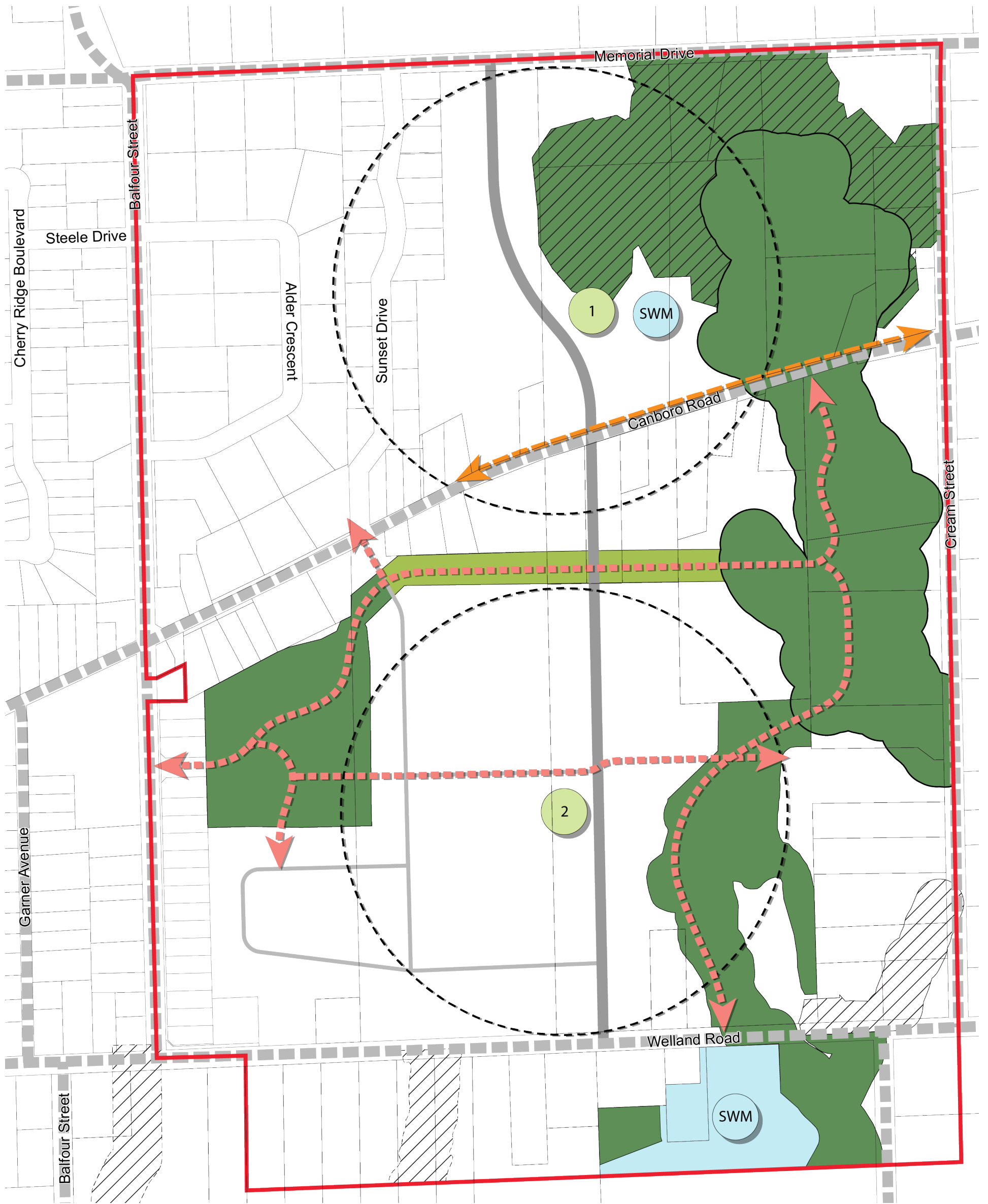
B1.9.13 Transitional Framework

- a) Preparation of the East Fenwick Secondary Plan was undertaken alongside a concurrent development proposal. Because of this, certain sections of the Plan Area will be regulated under policies and legislation that pre-date several substantial changes in the Town's planning landscape, including but not limited to:
 - i. The 2022 Regional Official Plan;
 - ii. The Provincial Policy Statement;
 - iii. Changes to the Environmental Impact Study process and requirements; and
 - iv. Amendments to the *Planning Act*.
- b) Notwithstanding Policy B1.19.13 a), the transitional policies of the 2022 Niagara Regional Official Plan and legislative framework applicable at the time of all future development applications in the Plan Area shall apply.















- Legend**
- Residential Neighbourhoods
 - Greenlands System
 - Environmental Linkages
 - Open Space / Parkette
 - Storm Water Management Facility
 - Special Policy Area (Subject to EIS Study)
 - 2.5 to 5-minute Walking Distance (200m-400m)
 - Existing Roads
 - Proposed Minor Collector
 - Proposed Local Roads
 - Secondary Plan Boundary

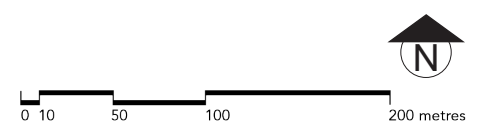
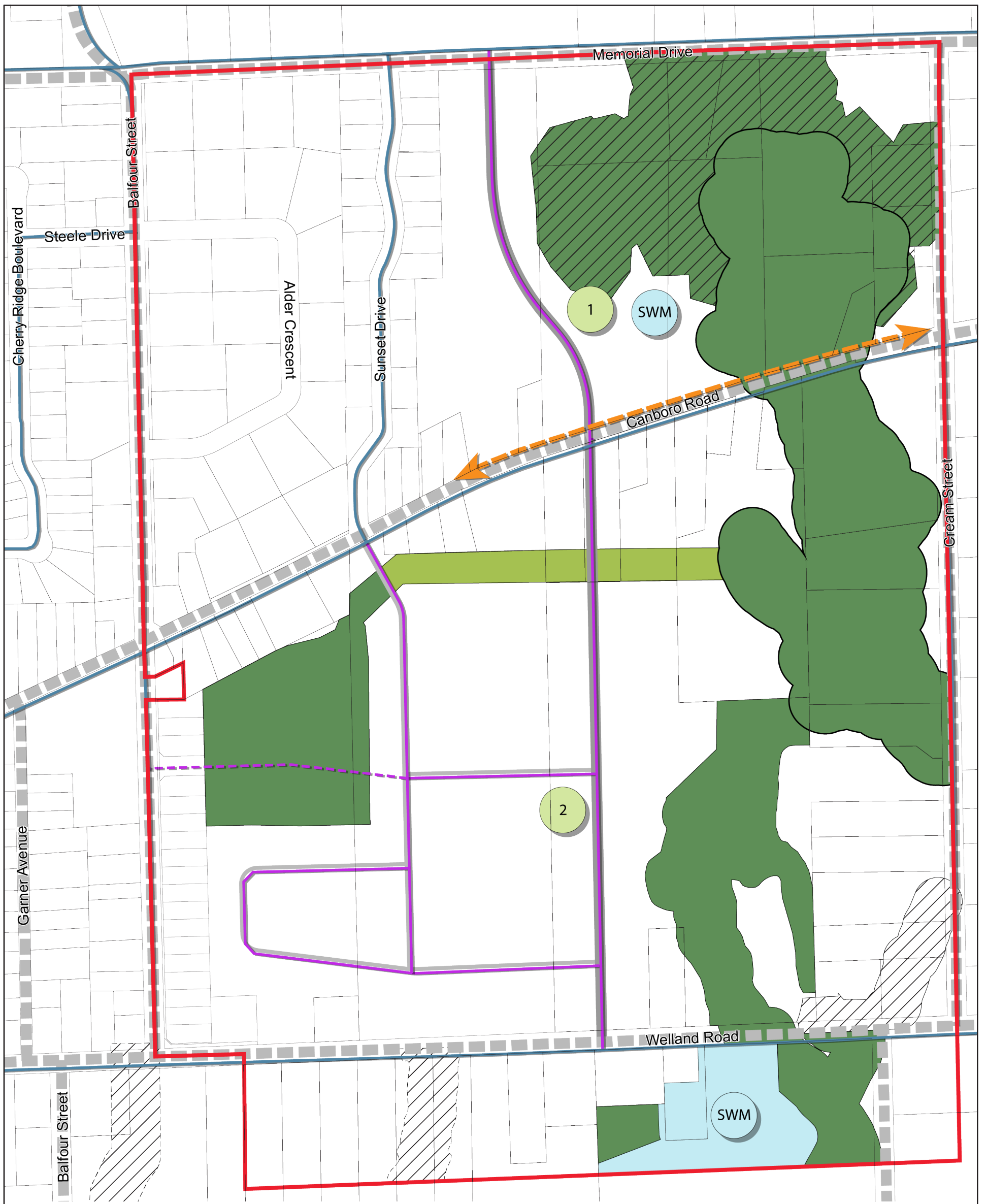
Schedule A7. East Fenwick Secondary Plan Area
Neighbourhood Structure Plan
April 2024
















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|---|--|---|---|
|  | Natural Heritage System |  | 2.5 to 5-minute Walking Distance (200-400m) |
|  | Environmental Linkages |  | Trail System (Subject to EIS Study) |
|  | Open Space / Parkette |  | Canboro Road Promenade |
|  | Special Policy Area (Subject to EIS Study) |  | Proposed Minor Collector |
|  | Stormwater Management Facility |  | Proposed Local Road |
|  | Existing Roads |  | Secondary Plan Boundary |

**Schedule A8. East Fenwick Secondary Plan Area
Greenlands Structure Plan**
April 2024

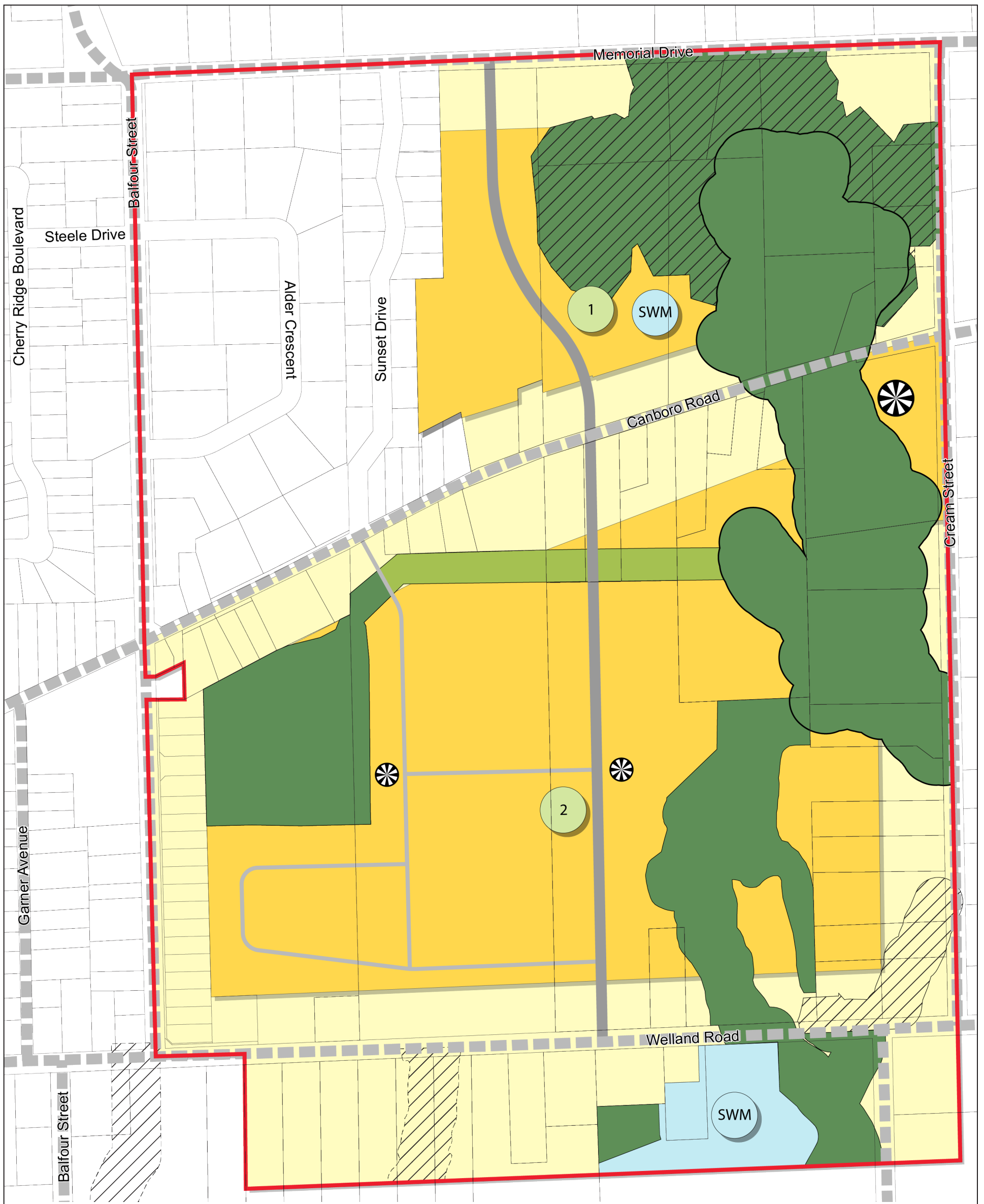


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|  | Greenland System |  | Existing Cycling Network |
|  | Environmental Linkages |  | Proposed Cycling Network |
|  | Open Space / Parkette |  | Proposed Off-Road Cycling Network |
|  | Stormwater Management Facility |  | Canboro Road Promenade |
|  | Special Policy Area (Subject to EIS Study) |  | Proposed Minor Collector |
|  | Existing Roads |  | Proposed Local Road |
| | |  | Secondary Plan Boundary |

**Schedule A9. East Fenwick Secondary Plan Area
Street Structure Plan**

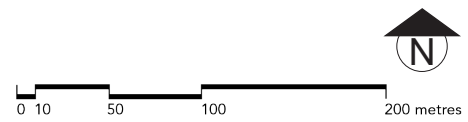
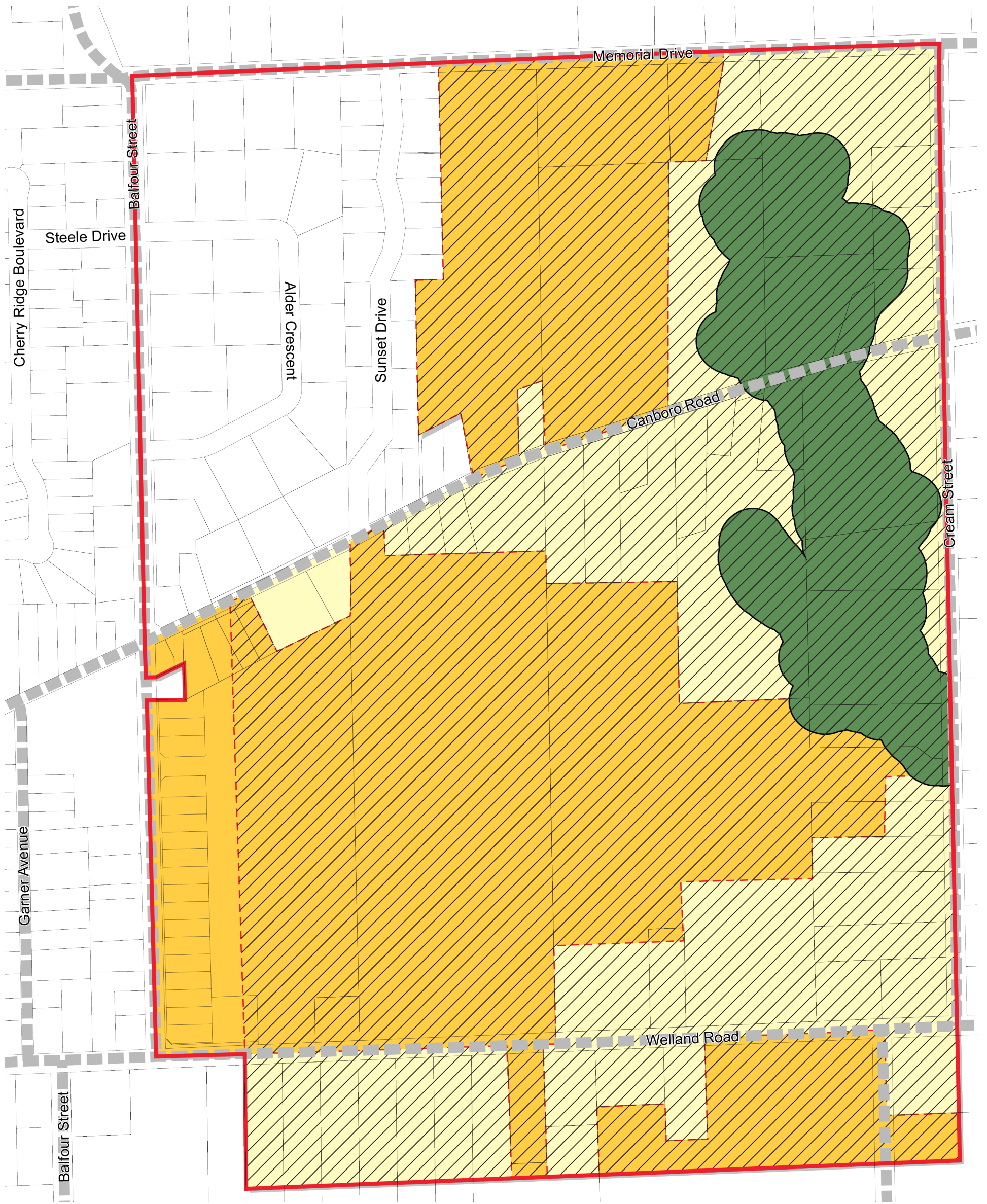
April 2024



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|--|--|--|--|
| | East Fenwick Village Residential | | Open Space / Parkette |
| | East Fenwick Neighbourhood Residential | | Stormwater Management Facility |
| | Natural Heritage System | | Lands Subject to Policy B.1.9.8.4.2 k) |
| | Environmental Linkages | | Proposed Minor Collector |
| | Special Policy Area (Subject to EIS Study) | | Proposed Local Road |
| | Existing Roads | | Secondary Plan Boundary |

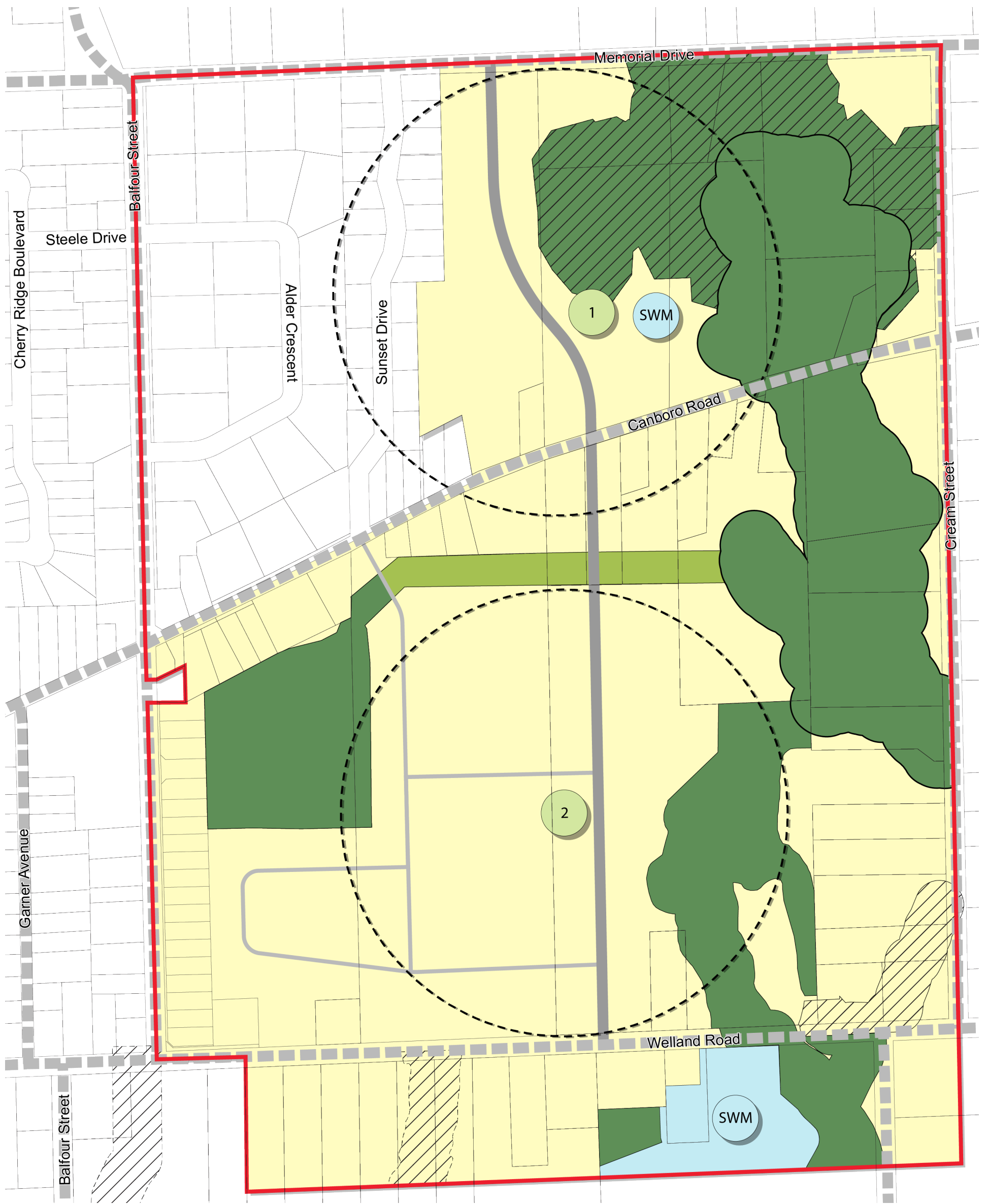
Schedule A10. East Fenwick Secondary Plan Area
Land Use Structure Plan
April 2024



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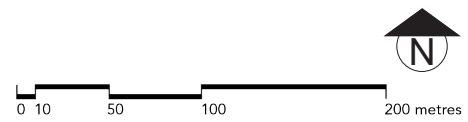
- Greenfield Overlay
- Greenfield Overlay (Subject to B1.1.10.3)
- Built Boundary (Subject to B1.1.10.3)
- Environmental Protection Three (EP3)
- Existing Roads
- Secondary Plan Boundary

Schedule A11. East Fenwick Secondary Plan Area
Official Plan Designations
April 2024



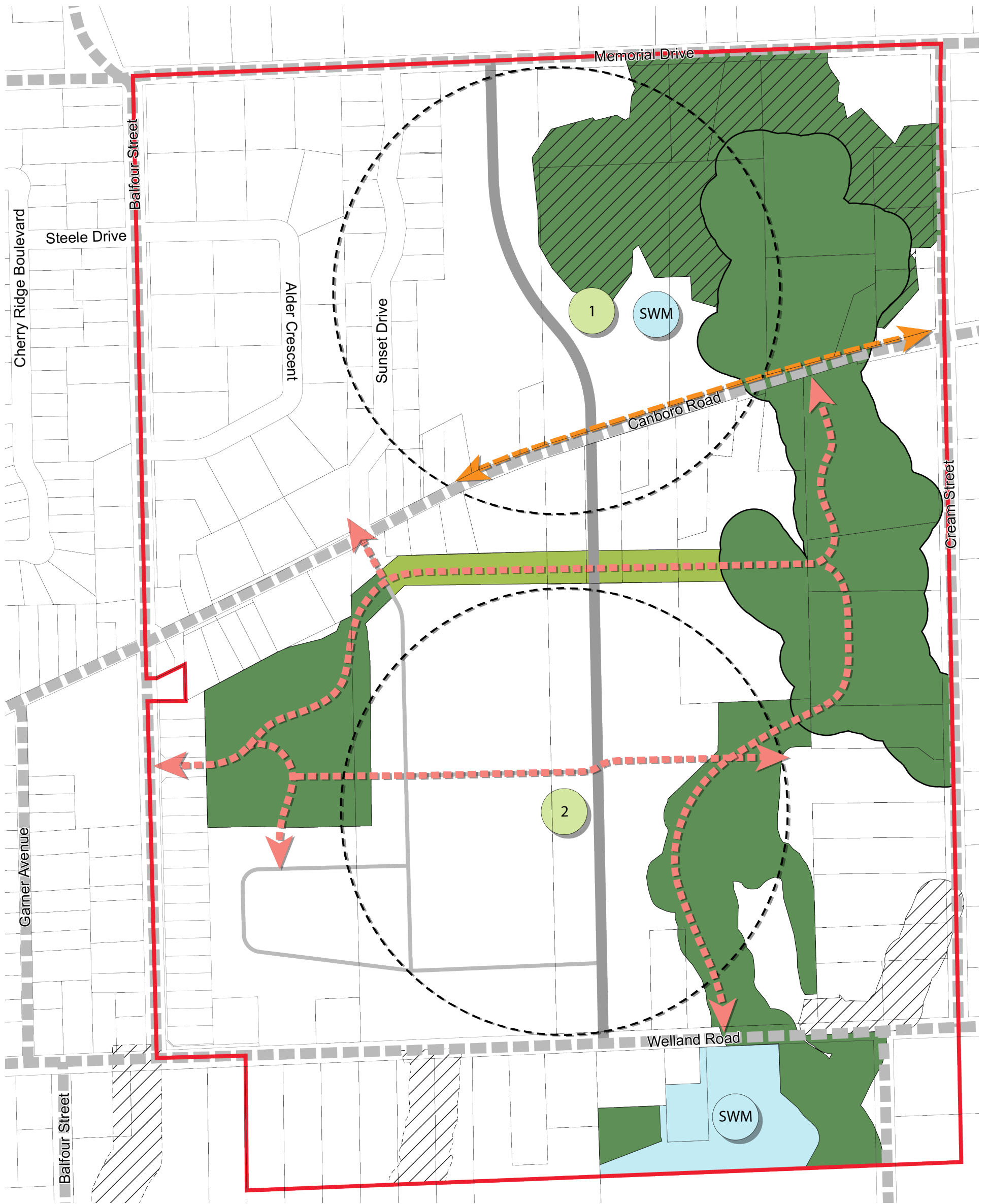
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| Residential Neighbourhoods | 2.5 to 5-minute Walking Distance (200m-400m) |
| Greenlands System | Existing Roads |
| Environmental Linkages | Proposed Minor Collector |
| Open Space / Parkette | Proposed Local Roads |
| Storm Water Management Facility | Secondary Plan Boundary |
| Special Policy Area (Subject to EIS Study) | |















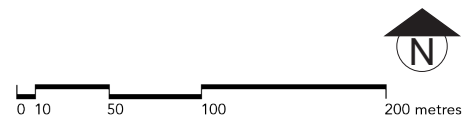
**Schedule A7. East Fenwick Secondary Plan Area
Neighbourhood Structure Plan**

April 2024

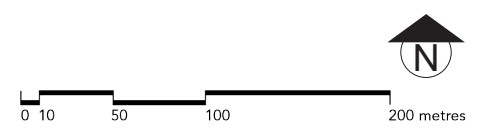
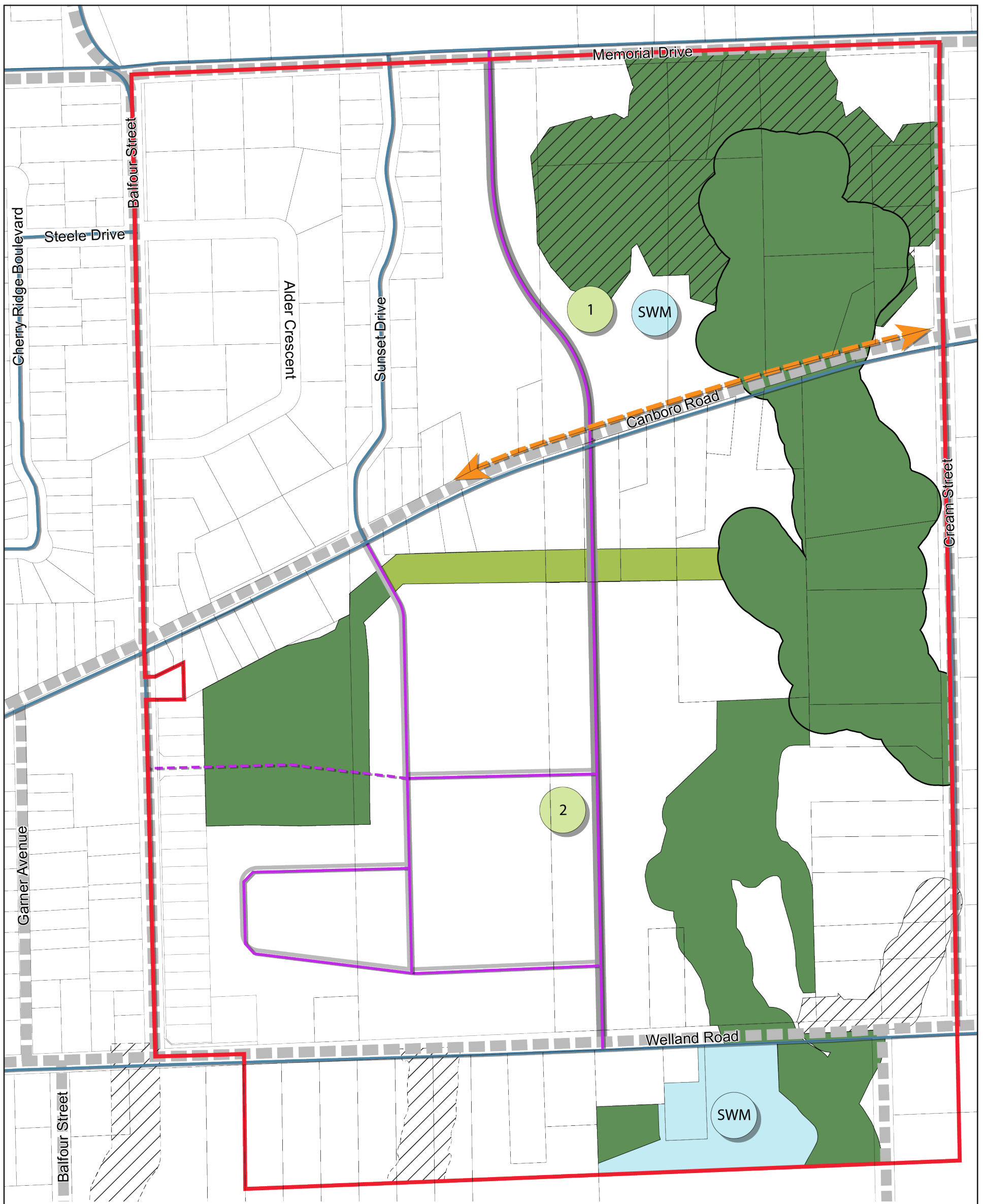


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




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|---|--|---|---|
|  | Natural Heritage System |  | 2.5 to 5-minute Walking Distance (200-400m) |
|  | Environmental Linkages |  | Trail System (Subject to EIS Study) |
|  | Open Space / Parkette |  | Canboro Road Promenade |
|  | Special Policy Area (Subject to EIS Study) |  | Proposed Minor Collector |
|  | Stormwater Management Facility |  | Proposed Local Road |
|  | Existing Roads |  | Secondary Plan Boundary |



**Schedule A8. East Fenwick Secondary Plan Area
Greenlands Structure Plan**
April 2024

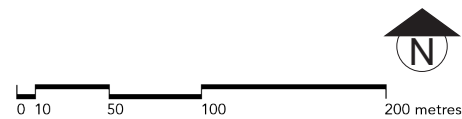
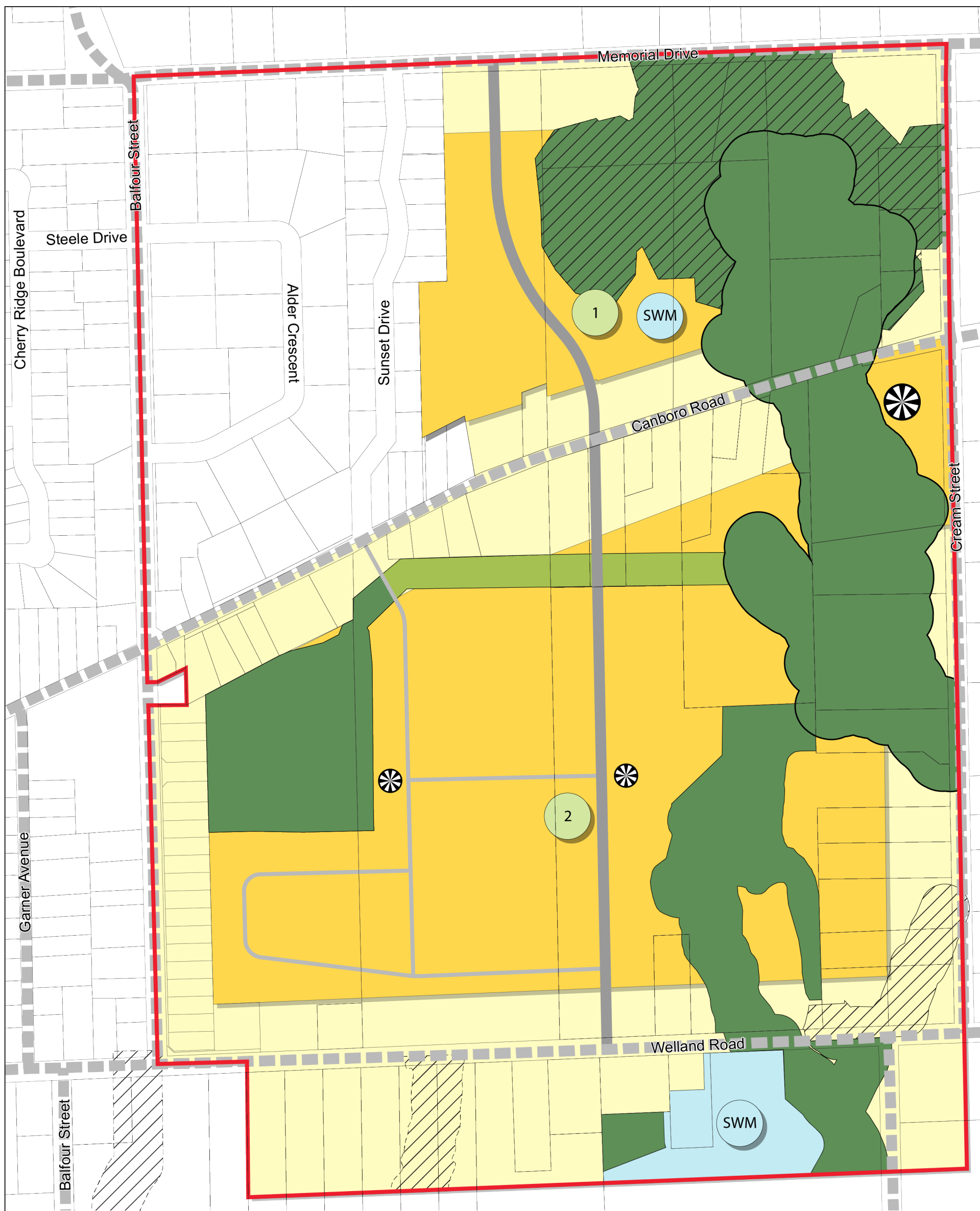


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|---|--|---|-----------------------------------|
|  | Greenland System |  | Existing Cycling Network |
|  | Environmental Linkages |  | Proposed Cycling Network |
|  | Open Space / Parkette |  | Proposed Off-Road Cycling Network |
|  | Stormwater Management Facility |  | Canboro Road Promenade |
|  | Special Policy Area (Subject to EIS Study) |  | Proposed Minor Collector |
|  | Existing Roads |  | Proposed Local Road |
| | |  | Secondary Plan Boundary |

**Schedule A9. East Fenwick Secondary Plan Area
Street Structure Plan**

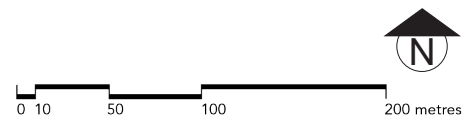
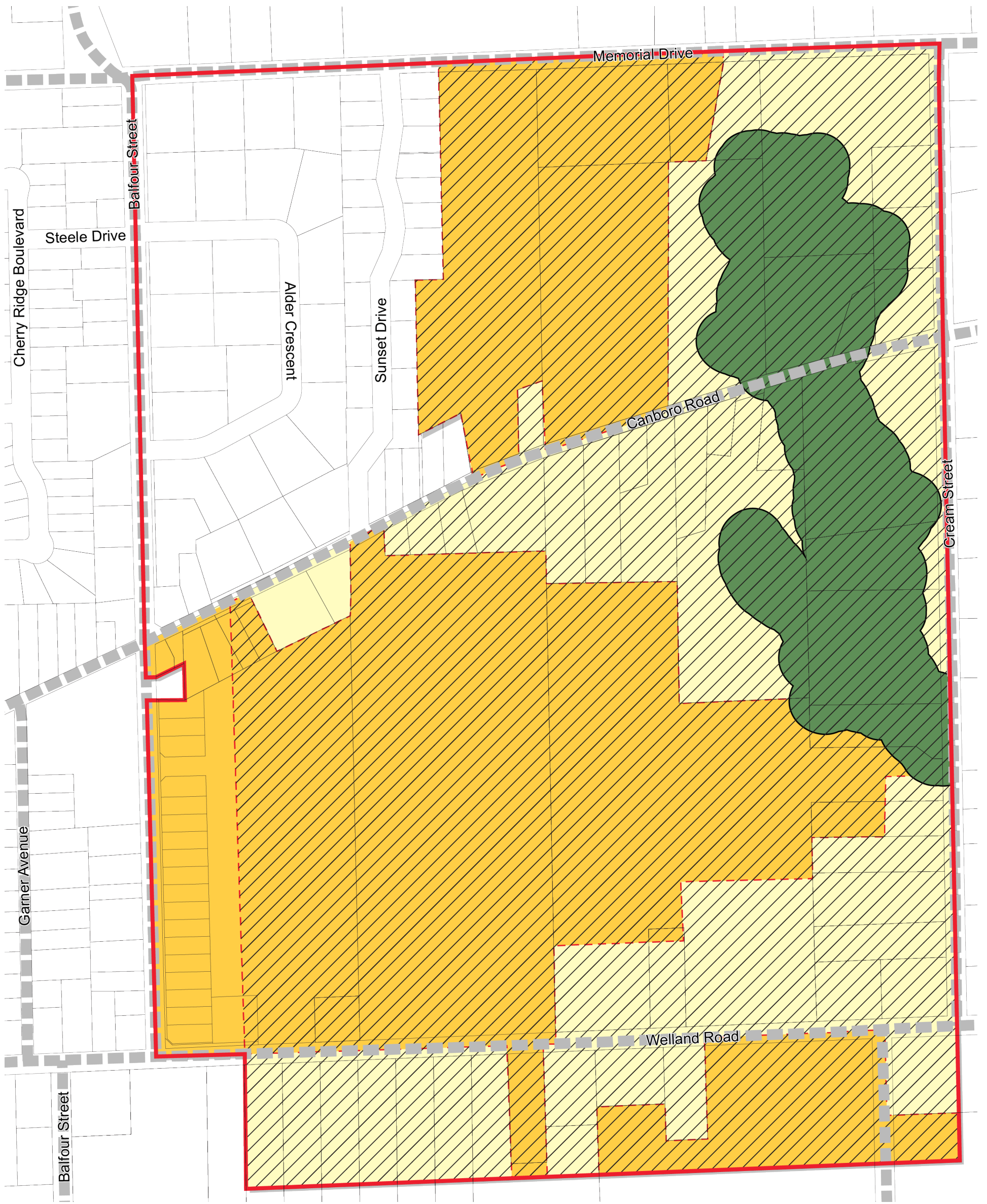
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- | | |
|--|--|
| East Fenwick Village Residential | Open Space / Parkette |
| East Fenwick Neighbourhood Residential | Stormwater Management Facility |
| Natural Heritage System | Lands Subject to Policy B.1.9.8.4.2 k) |
| Environmental Linkages | Proposed Minor Collector |
| Special Policy Area (Subject to EIS Study) | Proposed Local Road |
| Existing Roads | Secondary Plan Boundary |

Schedule A10. East Fenwick Secondary Plan Area
Land Use Structure Plan
April 2024



Legend

- Greenfield Overlay
- Greenfield Overlay (Subject to B1.1.10.3)
- Built Boundary (Subject to B1.1.10.3)
- Environmental Protection Three (EP3)
- Existing Roads
- Secondary Plan Boundary

Schedule A11. East Fenwick Secondary Plan Area
 Official Plan Designations
 April 2024



APPENDIX A - DEMONSTRATION PLAN & ACTIVE TRANSPORTATION PLAN





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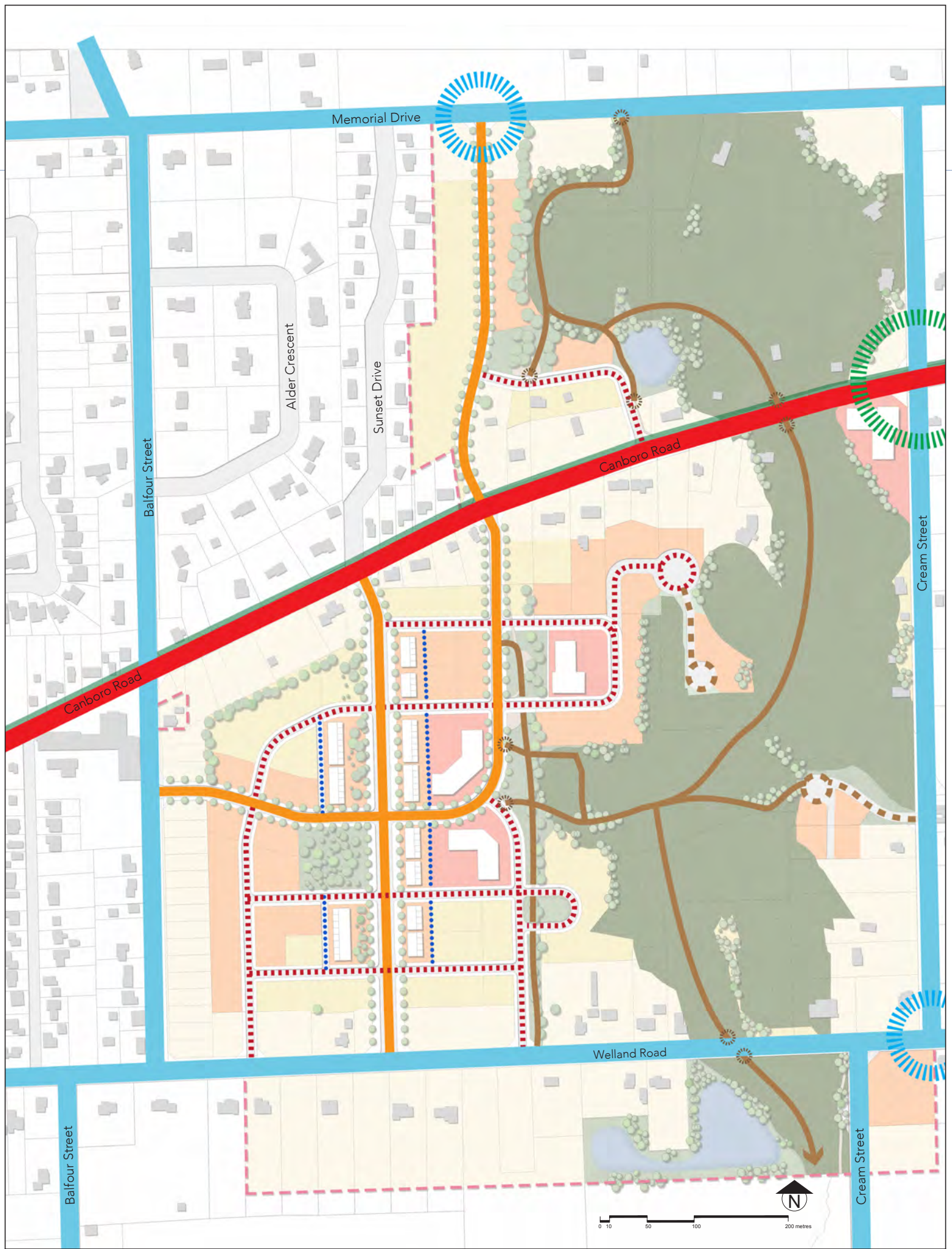


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- Village Residential
- Residential - Low Density (singles and Semis)
- Residential - Medium Density (Townhomes)
- Residential - High Density (Apartments)
- Open Space
- Natural Heritage
- Study Area Boundary

East Fenwick Secondary Plan Area
 Appendix A | Neighbourhood Demonstration Master Plan

June, 2018



Legend

- Gateway**
- Major Gateway
 - Minor Gateway
 - Trail Gateway

- Trails**
- Proposed off-road Trails

- New Street Network**
- Minor Collector
 - Local Road
 - Laneways
 - Private Road
 - Canboro Promenade

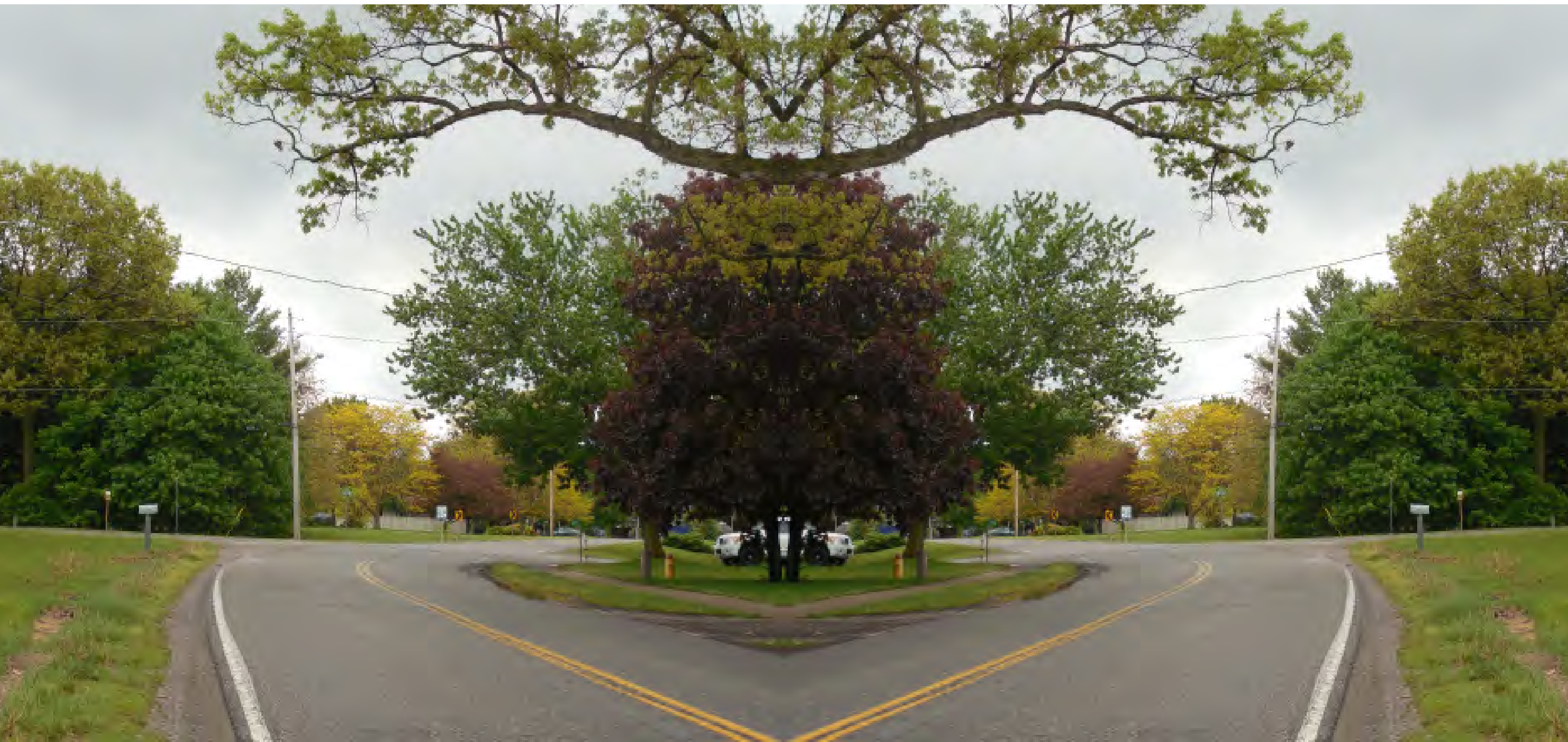
- Existing Street Network**
- Arterial Road
 - Collector Road

East Fenwick Secondary Plan Area
Appendix B | Active Transportation Plan

June, 2018



APPENDIX B - LAND USE BUDGET





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Fenwick Land Use Budget - GREENFIELD OVERLAY

Gross Greenfield Area (ha)	42.35
Natural Heritage System (ha)	9.59
Net Developable Area (ha)	32.76
Units Per Hectare	20
Unit Yield	655

Fenwick Land Use Budget - BUILT BOUNDARY SUBJECT TO POLICY B 1.1.10 OVERLAY

Gross Built Boundary Area Subject to Policy B1.1.	35.83
Natural Heritage System (ha)	15.19
Net Developable Area (ha)	20.64
Units Per Hectare	8
Unit Yield	165
Existing Residential Units	45

Remaining New Residential Units **120**

Unit Type	Description	NET Density (u/ha)	Percentage	units	Area	PPU	Persons	Jobs	P/J/ha	UNIT TYPE PERCENTAGE	
Large Singles	50x18	8	12%	78	9.72					52%	
Small Singles	33x12.5	18	41%	266	14.80						
Street Townhomes	33x7	31	28%	184	5.94					33%	
Live Work Units	33x7	31	5%	31	1			31			
Home Occupation @ 10% of units minus LiveWork. Assumes one person employment								63			
Walk-Up Apartments	132m ² average apartment size	75	15%	98	1.30					15%	
				100%	657	32.76	2.76	1,813	94	58	100%

Unit Type	Description	NET Density (u/ha)	Percentage	units	Area	PPU	Persons	Jobs	P/J/ha	UNIT TYPE PERCENTAGE	
Large Singles - Existing	50x18	8	29%	45	17.02						
Small Singles	33x12.5	18	43%	41	2.27					72%	
Small Singles	33x15.25	10	43%	0	0.00						
Home Occupation @ 10% of units minus LiveWork. Assumes one person employment								17			
Walk-Up Apartments	132m ² average apartment size	75	3%	84	1.12					3%	
				118%	170	20.41	2.76	469	17	24	75%

Total Potential Future Fenwick Popul: 2,674

Total Persons Study Area	2282
Total Jobs Study Area	111
Total Developable Area (ha)	53.3968
Total Persons and Jobs Per Hectare	43
Total Units Per Hectare	15.36

Density Assumptions - East Fenwick Secondary Plan

Unit Type	Lot Size	Net Net Lot Area (m ²)	Street & SWM Area		Apartment Unit Assumptions					
			assumption @ 35% (28% for Streets and 7% for SWM) (m ²)	Net Area (m ²)	Buildin g Size @ 22x60 average	Units/H a	Unit Yield per Floor @ 132m ² (Gross Area)	Floors	Total Unit Yield	
Large Single	50x18	900	315	1,215			8			
Small Single	33x12.5	412.5	144	557			18			
Townhouses	33x7.2	237.6	83	321			31			
Apartment	63x63	3,960	1,386	5,346	1,320	75	10	4	40	



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APPENDIX C - URBAN DESIGN GUIDELINES





ENVISIONING FENWICK

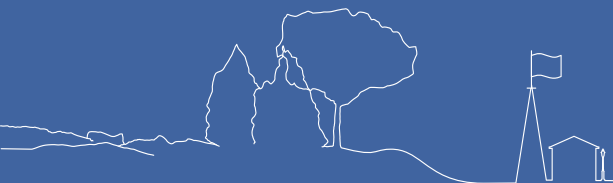
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EAST FENWICK SECONDARY PLAN URBAN DESIGN GUIDELINES

TOWN OF PELHAM

APRIL 2024



ENVISIONING FENWICK



PRIMARY CONTACT

Ute Maya-Giambattista

Principal

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Toronto, M5A 3X9

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ENVISIONING FENWICK

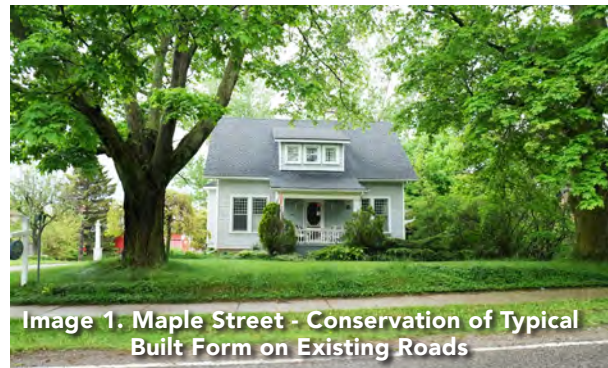
I. INTRODUCTION

1.1. PURPOSE AND SCOPE

All planning applications are to conform to the *Town of Pelham Official Plan* and the *East Fenwick Secondary Plan*.

This Guideline document intends to provide guidance and serve as an example of the key principles and policies addressed by the *East Fenwick Secondary Plan*, which has supplementary policies pertaining to Urban Design. These Urban Design Guidelines are based on the following design principles:

- a) Conserve, protect, enhance and integrate cultural and built heritage resources;
- b) Encourage a diversity of housing that compliments and enhances the Town of Fenwick's community character;
- c) Encourage compact, pedestrian friendly neighbourhoods that foster a healthy and inclusive community;
- d) Design attractive new communities with a distinct character, identity and sense of place;
- e) Develop a continuous and connected open space and trail system;
- f) Reinforce the function of the downtown as the primary business, entertainment and commercial point of the community;
- g) Promote active transportation and the use of open space to encourage tourism and attract residents seeking an active recreational community as a lifestyle or retirement choice; and,
- h) Incorporate sustainable design initiatives that enhance residential and local ecosystem's health and encourage the greening of streetscapes, parks, subdivisions etc.



These guidelines are not intended to replicate the policies within The *East Fenwick Secondary Plan* or the *Official Plan*, but to provide a greater level of guidance on urban design and sustainability. These guidelines recognize that variations to these provisions might be necessary to accommodate for specific site constraints. It is expected that the proponent in collaboration with Town staff will endeavour to identify the appropriate design solution based on the design best practices put forward by this document. A complete community is achieved through good urban design that complements sustainable development. The intention of this guideline document is to bring together key design elements that shape the built environment from both perspectives, and achieve the overall goal of creating a sustainable, complete community in the settlement of Fenwick. Mindful of that, the purpose is to provide a design vision and guidance for the *East Fenwick Secondary Plan* area by addressing the nature, intensity, quality and level of sustainability in both the public and private realms, while still ensuring that all other goals and objectives of the *Secondary Plan* and *Official Plan* are achieved.



Image 4. Reinforce the Distinct Character and Function of the Downtown



Image 5. The Village Niagara - Pedestrian Friendly Mid-block Connector



Image 6. Cornell - Develop a continuous and Connected Open Space System

1.2. DOCUMENT STRUCTURE

This report is organized into five main Sections:

Section 1: Introduction of the report, which provides background information and highlights the purpose of this report.

Section 2: The Land Use Plan outlines the overall vision for the *East Fenwick Secondary Plan* Area, and describes the built form and public realm components of the Land Use Plan.

Section 3: Built Form Guidelines address residential development and built heritage resources. They provide guidance for each of the residential land use designations within the *Secondary Plan* area and further define design guidance for building use, typology, setbacks, massing and transition, placement and orientation and treatment of the built form. Urban Design Guidelines address Built Heritage Resources within the Secondary Plan Area.

Section 4: Public Realm Guidelines address key components of the public realm, from the design of blocks to guidance on enhancing the existing street network as well as design guidance for new streets. This Section also addresses the integration of active transportation infrastructure and includes guidance for the Natural Heritage System (NHS) and coordinated open space network of parkettes, trail systems and stormwater management facilities.

Section 5: Green Infrastructure and Building provides further guidance on the public realm and built form by emphasizing sustainable technologies, resource efficiency, and responsible consumption. This Section addresses issues of energy efficiency, water conservation and management, material resources and solid waste, sustainable programs, and lighting.

The East Fenwick Urban Design Guidelines aim to supplement the policies and guidance from the *Region of Niagara's Official Plan*, the *Town of Pelham's Official Plan*, and the *East Fenwick Secondary Plan*. The design elements and guidance in this document address both the built form and the public realm and aim to offer tangible direction in order to deliver the principles and vision outlined in the East Fenwick *Secondary Plan*.

2. THE LAND USE PLAN

2.1. EAST FENWICK SECONDARY PLAN AREA VISION

The Vision for the *East Fenwick Secondary Plan* is based on the *Town of Pelham's Official Plan* and aims to deliver a new community that:

Supports and emphasizes Fenwick's unique character, diversity, civic identity, rural lifestyle and heritage features and to do so in a way that positively affects the quality of life and health for the citizens who live and work in Pelham.

The vision will be developed based on the policies in the *Official Plan* and the *East Fenwick Secondary Plan*. The vision intends to encourage and manage development in a manner that delivers a vibrant and diverse community. Integral to this vision is the design and integration of three distinctive sub-neighbourhoods, all of which will contain a variety of residential housing options and a distinctive open space.

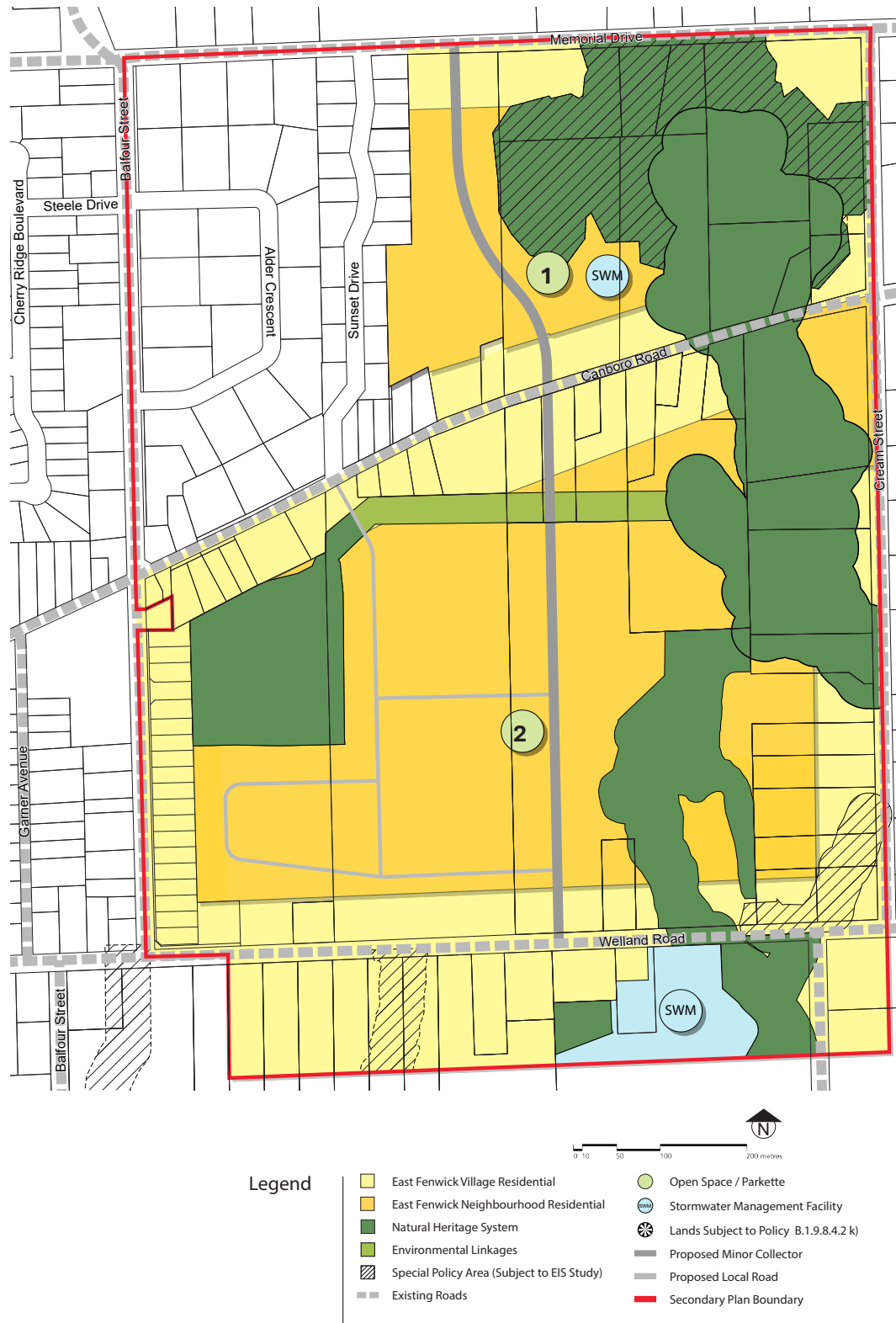


2.2. COMPONENTS OF THE LAND USE PLAN

The Land Use Plan (**Figure 1**) illustrates a structure for guiding future built form and public realm components for the *East Fenwick Secondary Plan Area*. It provides guidance on the density and building typology required to meet the Region's density targets. The primary objective of the built form components is to ensure that any new development reinforces a coherent, integrated, and compact built environment. The Land Use Plan also incorporates public realm features, ensuring the integration and coordination of an open space network and the NHS.



Figure 1. East Fenwick Land Use Plan



Schedule A10. East Fenwick Secondary Plan Area
Land Use Structure Plan

3. BUILT FORM GUIDELINES

3.1. RESIDENTIAL DEVELOPMENT

New residential developments within the *East Fenwick Secondary Plan* will integrate with the existing built form and rural character of the Town of Fenwick. Therefore, it is essential that future community and architectural design solutions address matters of area character fit, through the implementation of the design best practices contained in this document. The Urban Design Guidelines aim to reinforce the structure and image of Fenwick and provide general direction for housing typology, housing mix, building form and landscaping.

3.1.1. HOUSING MIX AND DIVERSITY

As per the *Niagara Region and the Town of Pelham's Official Plans*, the *East Fenwick Secondary Plan* is required to provide a combined density of 45 Persons and Jobs per hectare within both the Built Urban Area and the Greenfield Overlays. A mix and diversity of housing will be required to meet this density target. Therefore, the following guidelines apply:

1. A variety of architectural styles, elements, and material detailing should be considered to create distinctive and complementary character, as well as provide visual interest.
2. Live-work units provide an option for home-based employment, while also addressing the employment targets required for the *East Fenwick Secondary Plan*.
3. Universally accessible housing options should be provided to accommodate the widest spectrum of people to live in the community, regardless of age, ability and socio-economic status. Lifecycle housing options should be provided to support a variety of age groups, including houses designated especially for seniors (ex: bungalows).



Image 7. Tribute Communities - Semi Detached and Single Housing Types



Image 8. Cornell - Townhouse Housing Type



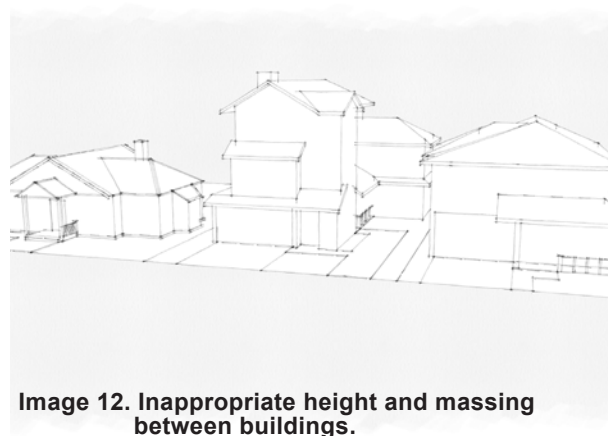
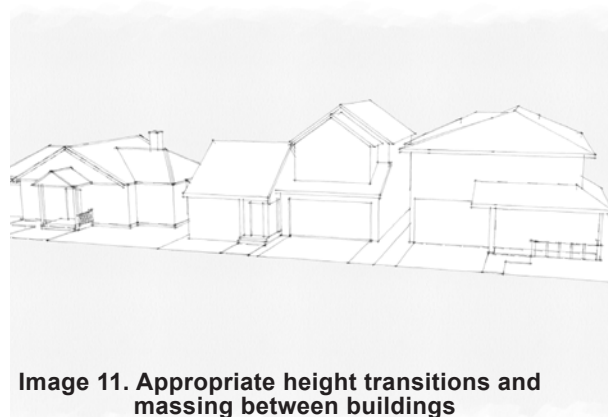
Image 9. Walkup Apartment Units

3.1.2. HOUSING TYPES

3.1.2.1 Village Residential

The following presents guidelines for single-detached and semi-detached homes within the Village Residential designation, as seen in **Figure 1**. Where other innovative development standards are proposed, they should be assessed to ensure that the intent and spirit of these guidelines are being met with respect to their primary objective.

1. Houses should be designed to frame the street edge with a consistent setback, and have front doors, windows, verandas, and entry features facing the road.
2. Where denser built forms and townhouse structures are permitted, appropriate transition to adjacent, existing built form should be considered. Character and architectural style of new structures should complement the existing built form, with respect to the scale, material and massing. In order to ensure that shadow and over-view issues are addressed new construction must step back and be scaled to the existing context, see images 11 and 12. Where such structures are permitted, garage areas must not be visible from the public right-of-way (see Section 3.1.5, lane-access garage, for more guidance).
3. The front yard setback of new single or semi-detached homes should complement the adjacent setbacks built form within the Village Residential designation. Therefore, the main building face should be set back within a range of 12.0 to 20.0 metres from the edge of the right-of-way.





4. Interior side yard setbacks should be:
 - a. A minimum of 1.0 metres on one side and 2.0 metres on the other side of the main building; and,
 - b. 2.75 metres on one side of the main building where the attached or detached garage is located in the rear yard and is accessed by a driveway crossing the front lot line.
5. Exterior side yard (corner lots) should be 2.4 metres.
6. For setbacks to garage buildings refer to Section 3.1.5.
7. Rear yard setbacks:
 - a. Rear yard setback to main buildings should be a minimum of 10.0 metres, with appropriate privacy fencing or greenery. Where feasible, the conservation of existing tree line should form a landscaped buffer. Where a tree line is non-existent, local trees or hedges should be planted to create the landscape buffer.
8. Encroachments into the front and exterior side setbacks such as porches, bay windows, canopies, and other features should be between 1.5 to 2.0 metres to add visual interest along the streetscape.
9. The front elevation of the house should be designed so that its front entrance design and architectural elements reduce the visual dominance of the garage and the front driveway. Garages shall not protrude beyond the main front wall of the dwelling unit.
10. Driveways should be designed to reduce the amount of pavement on front yards and enhance the visibility of the street. The use of semi-permeable or permeable paving materials and treatment is encouraged. These may be used to pave the entire driveway or as borders around the driveway to provide visual interest and enhance the permeability of water into the ground.
 11. Single detached and semi-detached homes should be one to two storeys in height.
 12. Where possible, the new building sitting should conform to the traditional angle in which the existing homes and lots on Canboro Road are positioned.
 13. A garage should not dominate more than 50% of the total facade of a home.
 14. Double car garages should have two single garage doors instead of one double wide door.

3.1.2.2 Neighbourhood Residential

a) Single Detached & Semi-Detached Homes

Guidelines for Single Detached and Semi-Detached units within the Neighbourhood Residential designation are consistent to those identified in Section 3.1.2.1 of this report. In addition, Single Detached and Semi-Detached units within the Neighbourhood Residential designation should comply with the following standards:

1. Front porch stair encroachments into the front setback are permitted to a limit of 0.3 metres of the front or exterior lot line;
2. Front porches should have a minimum of 1.2 meter set back to ensure livable space is available
3. For houses with an attached garage in the rear; a minimum amenity area requirement may be considered rather than a rear yard setback. Such amenity areas may include outdoor space such as a second floor deck or porch, and must be setback at least 3.0 metres from the front lot line; and,
4. Rear yard setbacks:
 - a. Rear yard setback to main buildings should be a minimum of 6.0 metres, but on long blocks larger setbacks and rear wall articulation may be required;
 - b. Rear yards adjacent to the Village Residential designation or existing dwellings off Sunset Drive shall be at least 10.0 metres deep with appropriate privacy fencing or greenery. Fencing should not exceed six feet in height. Where feasible, the conservation of existing tree line should form the landscape buffer. Where a tree line is non-existent, local trees or hedges should be planted to create the landscape buffer.



b) Townhouses, Stacked Townhouses and Back to Back Townhouses

1. The front yard setback to the main building face should be within a range of 3.0 to 4.5 metres from the edge of the right-of-way, and on collector roads shall be encouraged to be moved to the minimum.
2. Townhouse front porches should have a minimum of 1.2 meter set back to ensure livable space. Stacked and back to back townhouses are exempt from this.
3. Exterior side yard setbacks should be 2.4 metres.
4. Interior side yard separation distances, building wall to building wall, should generally be 1.2 metres to 1.8 metres.
5. Rear yard setbacks:
 - a. Street townhouse rear yard setbacks to main buildings should be a minimum of 7.5 metres, but on long blocks larger setbacks and rear wall articulation may be required. ; and,
 - b. Lots immediately adjacent to the NHS or public open space should have a rear yard setback to the main building of a minimum of 6.0 metres.
6. Where a lane is provided, the setback to the rear garage from the laneway should be a minimum of 0.6 metres from the lane right-of-way. For further garage setback guidelines refer to Section 3.1.5. This rule exists to avoid parallel parking between rear garages and laneways. Laneway based residential visitor parking is to be accommodated through on-street parking.
7. To ensure an attractive streetscape is delivered, architectural controls shall be developed to address detailed building design aspect such as: massing, grading differentials, elevation articulation, garage articulation, materials colour and quality, roof design



as well as the proposed siting strategy. An Architectural Control process can be implemented as part of the Draft Plan of Subdivision conditions and agreement phase to be coordinated between Town staff and the proponent.

8. Garages should be accessed from a rear lane for all street townhouse dwelling units with less than 6.0 metres frontage. Street townhouses with less than 6.0 metres frontage should have rear accessed garages in order to:
 - a. Ensure that the front entry and habitable ground floor space, rather than garages, are the dominant features of front façades;
 - b. Provide adequate space for front yard landscaping and street trees;
 - c. Provide adequate space for incorporation of utility metres and mechanical equipment; and,
 - d. Provide sufficient on street parking in front of the units.
9. Where garages are located in the front of the unit, they should be paired to allow for more substantial front yard green space. Garages shall not protrude beyond the main front wall of the dwelling unit.
10. Encroachments into the front and exterior side setbacks such as porches, bay windows, canopies, and other features should be between 1.5 to 2.0 metres to add visual interest along the streetscape. However, stairs are permitted to encroach to within a 0.3 metre of the front or exterior lot line
11. Outdoor amenity areas can be provided in a variety of forms including front verandas, rear yards, rear deck above the garage, roof-top deck, balconies or a design with similar intent.
12. For townhouses with an attached garage in the rear, a minimum amenity area requirement may be considered rather than a rear yard setback. Such amenity area may include outdoor space such as a second floor deck.



Image 18. The Village Niagara - Lane based Townhouses with diverse balcony configuration

c) Apartments and Condominiums

1. Apartment buildings should be oriented to front onto and address the public road, with front yard setbacks between 4.5 metres to 6.0 metres.
2. Primary building entrances should be located and oriented to public roads, and designed to be visible and accessible to the public.
3. Apartment or condominium buildings should have an angular plane of 45-degrees from a height of 13.5 metres or 4 storeys to all public right-of-ways to ensure an adequate built form transition with adjacent buildings and minimize shadow impacts on the surrounding streets.
4. Transition for apartment buildings abutting lower density built forms, open spaces, and the NHS will include a minimum setback of 7.5 metres from the property line and a 45-degree angular plane from a height of 10.5 metres or 3 storeys. This provides a lower building at the rear and a gradual transition from the rear property line.
5. When planning the constructing of two or more mid rise apartments ensure the entire block is designed to maximize clustering of servicing points, parking access and outdoors amenity spaces.
6. Where a public laneway abuts a site, the laneway may be included for the purposes of establishing the setback and angular plane.
7. The ground floor of an apartment or condominium building should be of human scale to ensure that the public realm adjacent to the built form is comfortable, inviting, and safe. Buildings that have at-grade units can provide private, individual entrances or private amenity spaces; privacy issues can be addressed through appropriate soft landscaping.
8. The ground floor of an apartment or condominium building might have a floor to ceiling height of 4.5 metres to accommodate for employment opportunities.
9. Rooftop mechanical equipment shall be screened from view through architectural design that reflects the building's façade treatment. Add-on screening elements such as lattice are prohibited.

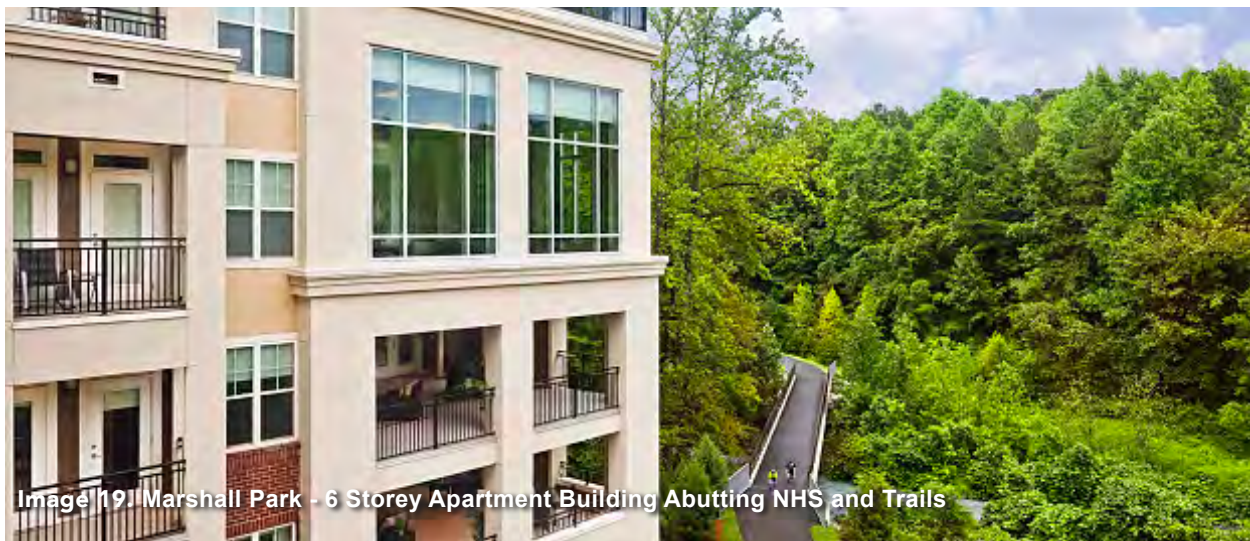


Image 19 | Marshall Park - 6 Storey Apartment Building Abutting NHS and Trails

10. Interior courtyards should be designed to maximize sun exposure through the massing and location of building elements.
11. Outdoor amenity area can be provided in a variety of forms including front verandas, roof-top deck, balconies or a design with similar intent.
12. A covered bicycle storage area should be provided at a rate of 0.3 per unit for residents and visitors.
13. All new apartment/condominiums shall provide indoor and secure bicycle parking and/or storage areas that are easily accessible to all residents. In addition, outdoor bicycle parking should be provided at the main entrance or if not feasible in a location that is in close proximity to the entrance and that is convenient and highly visible. Informational signage should be provided.
14. Vehicular parking facilities shall generally be located below-grade. Where this is not feasible surface parking facilities can be sited to the side or rear of buildings. The softening of the impact of surface parking through building orientation, use of permeable paving materials and landscaping treatments is required.
15. A visitor drop off area should be located at the side or rear of the building with lane access or a private drive.
16. Landscaping should be provided to differentiate areas and uses such as parking, building forecourts, courtyards, gardens, and sidewalks to give each site a distinctive, and clearly defined character.
17. Garbage and recycling areas should be screened from view by landscaping or an enclosure and should be located below-grade or at the rear of the building.
18. Service and refuse areas should be paved with an impervious surface materials such as asphalt or concrete.



Image 20. Cornell - Townhouses Abutting Open Space



Image 21. Scott Edwards Architecture - Human Scale Ground Floor

3.1.3. RESIDENTIAL DENSITY

1. Density plays a key role in determining housing form. The strategic allocation of density can contribute to compact form, increase to transportation efficiency and walkability within the community.
2. A mix of housing types will be required in order to meet the 20 unit per net hectare density target within the Greenfield designation.
3. The Built Boundary Area permits a density range of 8 to 15 units per net hectare. In order to promote compact development and conserve land, the top end of the permitted residential densities should be encouraged. A mix of housing types will also be required in order to meet this density target.
4. In order to meet the required residential and employment targets, as set out by the Niagara Region and Town of Pelham **Official Plans**, Live/Work Units will be permitted in the Neighbourhood Residential land use (**Figure I**). Urban Design Guidance for Live/Work Units fall under those in the Neighbourhood Residential designation.



Image 22. Semi-Detached Housing Creates Compact Development and Unit Diversity - Cornell

3.1.4. STREET INTERFACE

The interface between the public realm of streets and the private realm of developments is a transitional space that should be designed to promote and enhance walking and a sense of personal safety and comfort throughout this community. The following guidance pertains to the treatment of these interfaces.

1. Front entry elements should be articulated through the use of framing materials, colour and built form including porches, arches or articulated front steps.
2. Porches should be designed to be functional and useable. Porches should be deep enough to allow a seating area and a depth of 2.0 metres is highly encouraged, although a 1.8 metre minimum depth is allowed. Porches are permitted to encroach into the front yard setback.
3. Front porches should not be enclosed.
4. House entry features should be articulated through detailing and/or a variation of materials such as porticos and/or decorative treatments around the main door including stone, mill work, sidelights, accent lighting, or house number sign.
5. Appropriate native or xeriscaping front yard landscaping should be provided to enhance the overall streetscape quality and in turn, animate and create visual interest for pedestrians in the neighbourhood.
6. Decorative fencing around front and/or exterior side yards adjacent to the dwelling should not block the view of the sidewalk from the house. Their height should be limited to 1.2 metres and they should offer a level of transparency as primarily open structures, not solid walls. Solid, low fences can be used if made of local stone and fit with the general Fenwick character.
7. No two adjacent buildings should exhibit the same architectural style.

3.1.5. GARAGES & DRIVEWAYS

The design of garages and driveways can have a major impact on the visual character of the individual dwelling and the collective streetscape. These guidelines intend to enable a cohesive streetscape where attached garages and driveways complement instead of dominate the streetscape.

a) Front Garages

The following guidelines shall be applied in order to minimize the presence of attached and detached garage buildings accessed from the front of the lot:

1. Garages must be a natural extension of the design, massing, and material of the main dwelling.
2. For buildings within the Village Residential designation, garages should be set behind the main building face notwithstanding the presence of a porch feature.
3. Within the Neighbourhood Residential designation, garages for single attached, semi-detached or townhouse typologies should be set behind or flush with the main building face. Garage doors facing a public road should be setback a distance of at least 6.0 metres from the road right-of-way. For apartment or condominium dwellings, residential parking should be set behind the building.
4. Attached garages should be setback 6.0 metres from the rear lot line and 1.2 metres from the side lot line.
5. A variety of garage door configurations and styles should be provided. The door should have a maximum width of 2.4 metres for single doors, and 4.6 metres for one-and-a-half garage doors.



6. The maximum width of the garage door that faces the street (excluding the width of piers), should be no larger than:
 - a. A single car garage door for lots with less than 9.0 metres frontage;
 - b. A one and a half car garage door for lots between 9.0 metres and less than 11.0 metres frontage; and,
 - c. A two-car garage door for lots with 11.0 metres or a greater frontage where the garage door may be provided as one door or two separate doors provided in total they equal a two-car garage door. Double car garages should be comprised of two single garage doors separated by a masonry column or, in the case of full double door garages, style with the appearance of 2 single bay doors and a centre pier should be encouraged.
7. Tandem garage designs are encouraged to help minimize the impact of garage width on the elevation and in turn on the streetscape.
8. Glazed door panels are encouraged on all garage doors.

b) Lane-Accessed Garages

Garages that are accessed from a laneway can either be detached or attached to the main dwelling at the rear. Attached garages can either be set into the house with access at the rear, or they can be attached to the main dwelling through a breezeway, which forms a side courtyard for amenity space.

1. The minimum setback for garages accessed by a lane should be 0.6 metres from the lane right-of-way.
2. Side yard setbacks should be 1.2 metres minimum if the garage building has doors and/or windows other than the main vehicular entrance facing the side yard.
3. Side yard setbacks should be a minimum of 0.3 metres if the garage building has no doors and/or windows other than the main vehicular entrance facing the side yard but may be 0.0 metres where the garages on abutting lots are attached.
4. The garage door facing onto a laneway is not required to have a maximum door width.



Image 26. Oakville - Lane-Accessed Garage



Image 27. Lanefab - Laneway Secondary Suite

5. Where possible, garages should be paired to allow for increased rear yard, or an outdoor parking pad to accommodate resident parking.
6. The maximum number of attached garages on adjacent lots should be 4.
7. Secondary suites located above detached garages are encouraged for lot sizes greater than 6.0 metres, and should be located on end units.

c) Driveways

1. Driveway widths should generally be no larger than the interior width of the garage.
2. Driveways are encouraged to be paved with light-coloured or permeable materials to reduce storm water run off and reduce heat island effect.
3. Driveways should be located as far as possible from parks, open space features, public walkways, schools and intersections.
4. Below grade garages that are located below the elevation of the centreline of the road are discouraged.

3.1.6. PRIORITY LOTS

Priority Lots have high public exposure, such as corner lots or lots located adjacent to public open space. The identification and treatment of Priority Lots should be determined through the Draft Plan Subdivision process and Site Plan Agreement. The following provides guidance on the allocation of such lots.

a) Buildings facing and flanking arterial or collector roads

1. Units facing or flanking onto arterial or collector roads should be given special consideration in architectural design, massing, orientation, and siting. Materials should be of high architectural quality.
2. For dwellings flanking an arterial or collector road, the main front door should be visible from, and oriented to, the exterior side elevation of the house with access to the sidewalk. The entries should be articulated through the use of entry features such as projecting porches facing the street.
3. Side elevations flanking arterial or collector roads should be consistent with the front elevation in terms of materials, fenestration style and detailing.
4. Façades should be highly articulated through coordinated fenestration, masonry detailing, accent gables, dormers, and/or other special treatment.

b) Buildings Adjacent to Parks and Open Space

1. Front, side and rear elevations exposed to public spaces such as parkettes, should be well articulated. Articulation of buildings may also include changes in material colours and texture, changes in building plane, and variation of roof lines. The rear portions of buildings may benefit from rear yard covered porches. A combination of fenestration, bay windows, material changes and dormers may be used in addition to other design elements to achieve the objective.



2. Side and rear elevations should have a similar design and use materials that are consistent in architectural design and material with those used on front elevations. Architectural detailing such as corbelling should continue from front to side elevations, where visible to the public.
3. For units flanking onto parks, parkettes and public spaces, a highly articulated side face is encouraged. Architectural design and material should match the front facade's quality. Side main entrances are an alternate means to achieve this.
4. The location of porches, windows, and entry doors for units surrounding parkettes should maximize opportunities for overview and safety.
5. Projecting porches should emphasize the entrance as well as to reduce the presence of the garage.
6. Driveways of adjacent homes should be located as far away as possible from public space.
7. It is encouraged that medium to high density housing typologies are located adjacent or nearby open space so as to provide those residents with access to green space as well as increase the opportunities for social surveillance.

c) Corner Units

1. Side or rear elevations visible from the street should have windows, materials, and other architectural treatments equal in quality to the front elevation of the house.
2. Corner windows and wrap-around porches should be included to emphasize a corner location. Where possible, the entry door should be located on the exterior side elevation of the house with direct access to the sidewalk.
3. Townhouse units should be encouraged as bookends to residential blocks.



Image 28. Cornell - Articulated Façade Abutting Public Space



Image 29. The Village Niagara, High Architectural Quality with an Emphasis on Main Entrance Over Garage at the View Terminus of a T Intersection.

d) Gateway Corner Units

Gateway corner units are typically homes that are located at the entry to the community from adjacent areas. These units should be designed with the following principles in mind:

1. Gateway dwellings should be given special consideration in architectural design, massing, orientation, siting and materials, and shall be of high architectural quality.
2. Entry elements and porches are encouraged to produce interest in the façade as well as to help define the entrance to the neighbourhood.
3. Pairing of similar model dwellings on lots directly opposite each other is encouraged to establish and enhance a gateway condition.
4. It is encouraged that landscaping and landscape features are provided as a means to accentuate the unit and create an inviting atmosphere.
5. Low apartment blocks should be considered at key focal areas such as intersections, fronting parks etc.

e) T Intersections/Key View Terminus

T intersections occur when one road terminates at right angles to another. Consideration should be given to homes at the top of the T intersection and the last two lots on either side of the road that terminates at the intersection.

1. Architecture on lots at the end of T intersections should have facade designs that utilize elements such as coordinated fenestration, masonry detail, and entry elements.
2. Pairing of side yards is encouraged to form a landscaped area at the terminus of the T intersection.
3. Buildings sited at the end of the view corridor should be designed with architectural elements that address these views.



3.2. HERITAGE RESOURCES

3.2.1. CANBORO ROAD

The Canboro Road Corridor has been identified as an important historic scenic route and grouping of heritage resources in the *Town of Pelham Heritage Master Plan* (BRAY Heritage 2012). It has also been identified as a significant cultural heritage landscape (ASI, 2017). Key character-defining elements of the corridor include its diagonal alignment, which comes from an old Indigenous trail. It also has mature roadside vegetation, cemeteries and community buildings, which create a scenic route that connects Downtown Fenwick with Fonthill. The *Town of Pelham's Official Plan* identifies the Canboro Road Corridor as a special district in the Town of Pelham. Section B2.2.7 of the *Official Plan* identifies the corridor as an area of significant potential for enhancement as a rural promenade characterized by public parks and spaces geared to pedestrians and cyclists, as well as the promotion of agricultural based tourism and accessory commercial uses. As per *Official Plan* policy A2.4.2, Canboro Road shall promote an active transportation linkage between Fonthill and Fenwick. Canboro Road Corridor has also been defined as a Community Improvement Plan study area, should Council wish to foster and enhance this area as a promenade and tourist destination. This is the basis for the corridor enhancements proposed in the *Secondary Plan*, as per Section 4.1.2.1.

In addition to these policies, the following guidelines apply to development and road enhancements along Canboro Road:

1. The built form in this area should be consistent with the character, massing and setback of the existing built form.
2. Any buildings that do not conform to the single or semi-detached building typology shall be designed as a landmark building that fits within Fenwick's Village character and shall not exceed 4 storeys in height.
3. Garages and access to parking for new development may be located at the rear of the property in order to maintain the rural landscape character and enhance the walkability of the area. Attached garages are permitted and shall be set back from the main façade of the building.
4. Enhancements to the road should seek to maintain the village character by incorporating landscaping, bioswales and at-grade pedestrian walkways.
5. Sightlines at existing and new intersections should be enhanced to ensure a safe vehicular, pedestrian, and cycling thoroughfare.
6. Where possible, retain the existing tree canopy and mature vegetation and maintain the existing alignment of the road.
7. Enhancing the urban tree canopy should be encouraged when possible.

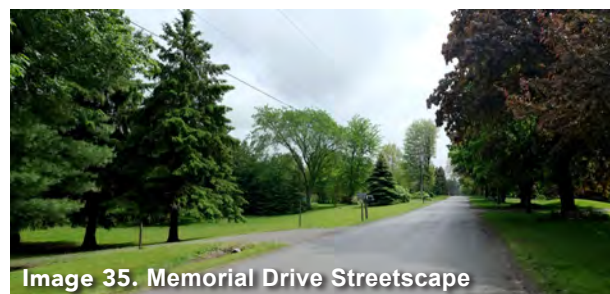


Image 32. Canboro Road Existing Streetscapes

3.2.2. **MEMORIAL DRIVE**

Memorial Drive is the northern boundary of the *East Fenwick Secondary Plan* area. It operates as a local roadway and a transition between the urban boundary and the rural areas to the north. Memorial Drive has low to medium volume of vehicular traffic, which moves at slow traffic speeds. It is an undivided paved corridor with an east-west orientation. Memorial Drive is located adjacent to the study's Provincially Significant Wetland, which is reflected in the natural landscape and mature vegetation adjacent to the road. It is identified as a significant cultural heritage landscape.

1. The built form in this area should be consistent with the character, massing and setback of the existing built form and building performance standards of Section 3.1.2.1.
2. Any buildings that do not conform to the single or semi-detached building typology shall be designed as a landmark building that fits within Fenwick's Village character and shall not exceed 4 storeys in height.
3. Garages and access to parking for new development may be located at the rear of the property in order to maintain the rural landscape character and enhance the walkability of the area. Attached garages are permitted and shall be set back from the main façade of the building.
4. Enhancements to the road should seek to maintain the village character by incorporating landscaping, bioswales and at-grade pedestrian walkways.
5. Sightlines at existing and new intersections should be enhanced to ensure a safe vehicular, pedestrian, and cycling thoroughfare.



3.2.3. CREAM STREET & WELLAND ROAD

The areas adjacent to these two streets are identified as having a single-family residential infill character. A number of cultural heritage resources have been identified adjacent to Cream Street and Welland Road, however the area has not been identified as a cultural heritage landscape. The land use is primarily residential, typically 1-3 storeys with a variety of architectural styles, consistent with the development pattern of gradual infill. The streetscape has a rural character with little or no shoulder; a few ditches, and no sidewalks or curbs. The following design guidelines should be considered:

1. The built form in this area should be consistent with the character; massing and setback of the existing built form.
2. Any buildings that do not conform to the single or semi-detached building typology shall be designed as a landmark building that fits within Fenwick's Village character and shall not exceed 4 storeys in height.
3. Garages and access to parking for new development may be located at the rear of the property in order to maintain the rural landscape character and enhance the walkability of the area. Attached garages are permitted and shall be set back from the main façade of the building.
4. Enhancements to the road should seek to maintain the village character by incorporating landscaping, bioswales and at-grade pedestrian walkways.
5. Enhance the sightlines at existing and new intersections to ensure safe vehicular, pedestrian, and cycling thoroughfare.



Image 36. Cream Street Streetscape



Image 37. Cream Street Built Form



Image 38. Welland Road Streetscape and Built Form

4. PUBLIC REALM GUIDELINES

4.1. DEVELOPMENT BLOCK DESIGN AND STREET NETWORK

4.1.1. BLOCK DESIGN

As per the *Region of Niagara's Urban Design Guidelines*, block length and design define and structure neighbourhoods, influencing development opportunities, movement options and neighbourhood character. Each guideline may not be appropriate for all instances and an alternative may be better suited for a specific development. Therefore, variations to these provisions may be considered by Town staff, when it is demonstrated that an exception may be appropriate, provided that adjustments to block lengths maintain and facilitate pedestrian movement throughout the community through the provision of clear and safe mid-block connectors and walkways. The following are design guidelines for block design for East Fenwick:

1. To maximize connections and permeability for both vehicular and active transportation modes, the street should be based on a grid pattern that is modified in response to natural open space, built heritage or existing street conditions.
2. Block lengths should generally range between 150 to 250 metres to promote walkability.
3. New developments that are adjacent to existing built form should pursue opportunities to integrate with or connect to existing streets. The number of connections should be maximized to increase permeability.
4. Blocks longer than 250 metres may be considered on an individual basis where other block alternatives are not feasible. In such cases, publicly accessible pedestrian walkways (refer to Section 4.2.1) should be provided to facilitate pedestrian circulation and access.

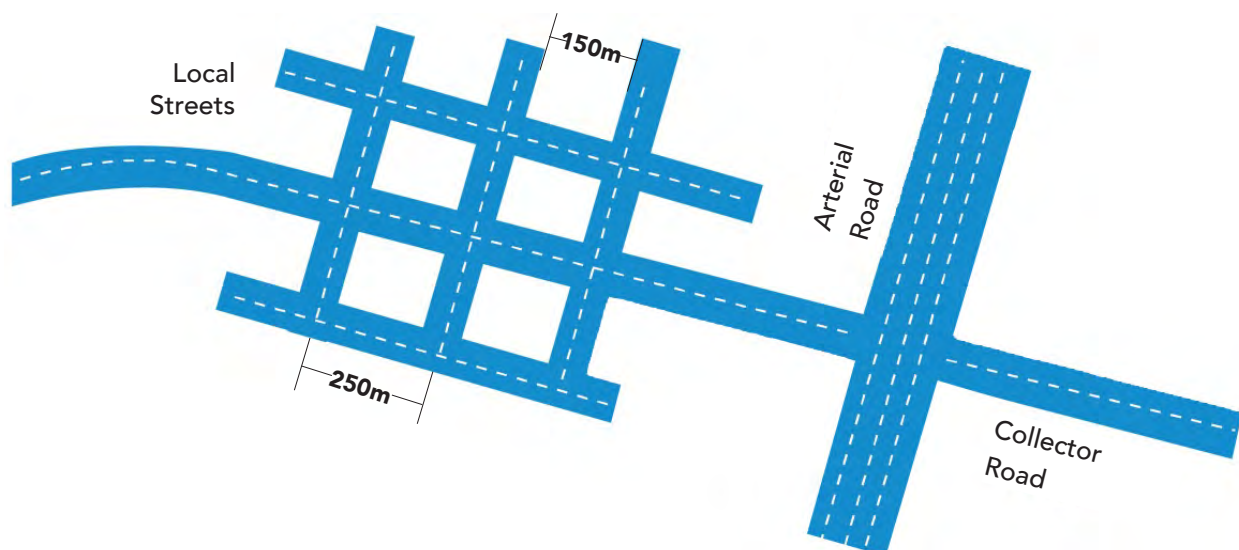


Figure 2. Block Design Adopted From Congress for New Urbanism



5. Draft plans shall be designed to ensure that residents are generally located within a 200 to 400 metre radius (2.5 to 5 minute walk) to a parkette or to other passive recreational elements such as trail heads, neighbourhood park, community park, or school.
6. Where possible, existing hedgerows should be conserved and integrated into the neighbourhood's local street and block pattern. As per **Secondary Plan** policy BI.9.11.2.f, Heritage Greenways should be within the public right of way, adjacent to the pedestrian realm.
7. In order to minimize the visual impact of long blocks, lots located at the end of blocks should be turned 90-degrees to face the other road, where appropriate. However, a variety of lot facing conditions, in addition to flankage lots, should be considered along long stretches of collector and arterial roads.
8. Where rear lanes are used, the maximum lane length should be no more than 180 metres in order to provide for a maximum 90.0 metre hose length from fire hydrants located on road connections. Where the 180 metre length is exceeded, an on-street hydrant location and fire hose access to the rear lane shall be provided.
9. A block of lots may front onto a public open space, such as a parkette, provided the rear lot line adjoins, and has access from a rear lane or a public road.
10. The use of cul-de-sacs should be minimized, except where necessary due to grading and topography or at view terminus sites. Where cul-de-sacs are used a pedestrian and/or bicycle through-connection should be provided to promote active transportation.
11. Significant hedgerows within public open space or parks should be protected where possible and feasible. Any grading activities around the hedgerows should be minimized.



4.1.2. STREETS

The *East Fenwick Secondary Plan* policy states that where appropriate and feasible, arterial and collector roads streetscape design will include sidewalks on at least one side of the street; on-road or separated bicycle lanes and trail connections.

Standard street design guidelines put forward by this document include:

- The careful evaluation of the alignment of streets adjacent to existing hedgerows and wooded areas to ensure the site's existing natural features are visible and part of the community's open space network;
- The careful evaluation of the retention or realignment of existing drainage features subject to NPCA approval. If an existing drainage area is to be realigned it should be incorporated into the street's cross Section in the form of bio-swales or other features that continue to support the area's drainage. (See Boulevard B cross Section under Street and Block Network Section). The design of the realignment should consider measures to prevent risk of drowning;
- New streets must be designed to accommodate the infiltration of stormwater runoff from the 25 mm storm event through the use of roadside ditches and medians to meet the Stormwater Reduction Targets;
- Where feasible, a 2.5 metre on-street parking width is recommended, specially where bike lanes are placed beside on-street parking;
- Streets should have a relatively consistent building edge in order to provide spatial definition and containment to the street. A minimum and maximum front and exterior setback zone will be an important component of this strategy;
- The implementation of a rural-cross Section for new deciduous streets with street trees and swales as an essential component of their engineering and an essential component of the community's streetscape character; and,
- A utility strategy where utilities are clustered or grouped where possible to minimize visual impact.

4.1.2.1. Existing Street Enhancement

The following Section describes the proposed design standards and enhancement measures for each existing road topology within the East Fenwick study area.

a) Canboro Road

As one of the most iconic scenic routes in Pelham, Canboro Road, is the spine linking Fonthill to Fenwick and is envisioned as a future pedestrian and cyclist friendly corridor. Canboro Road, located at the heart of the *East Fenwick Secondary Plan* area is designated as an Arterial road in the *Official Plan*.

Three design cross-sections were developed for the road recognizing its reduced width (12.0 metres at its narrowest) and the potential need to widen the road, should better pedestrian and cycling facilities be introduced.

Option 1 shows a streetscape character composed of:

- A total right of way width of 19.0 metres;
- Two vehicular traveling lanes;
- On-street cycling lane on both sides of the road;
- Landscaped swale/boulevards with no street curb on both sides of the road; and,
- A pedestrian sidewalk on the north side of the road.

Option 2 shows a streetscape character composed of:

- A total right of way width of 21.0 metres;
- Two vehicular traveling lanes;
- On-street cycling lane on both sides of the road;
- On-street parking on north side of the road only;
- Landscaped swale/boulevards with no street curb on both sides of the road; and,
- A pedestrian sidewalk on the north side of the road.

Option 3, our recommended strategy as it implements a pedestrian Promenade along the road, is composed of:

- A total right of way width of 24.0 metres;
- Two vehicular traveling lanes;
- On-street parking on both sides of the road;
- Landscaped swale/boulevards with no street curb on both sides of the road; and,
- A two-way cycling trail and pedestrian sidewalk Promenade on the north side of the road.



Image 40. Proposed Enhancement of Canboro Road

Figure 3. Canboro Road - Option 1

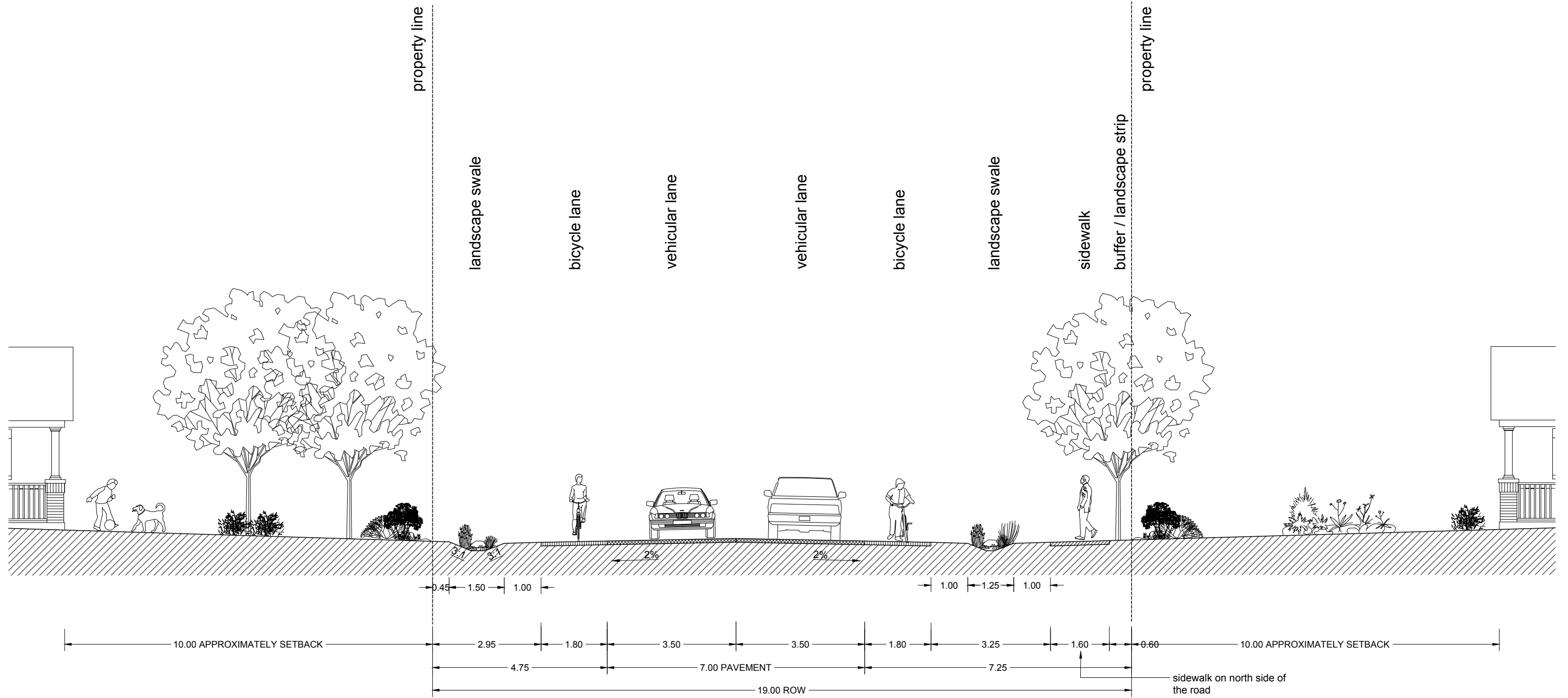


Figure 4. Canboro Road - Option 2

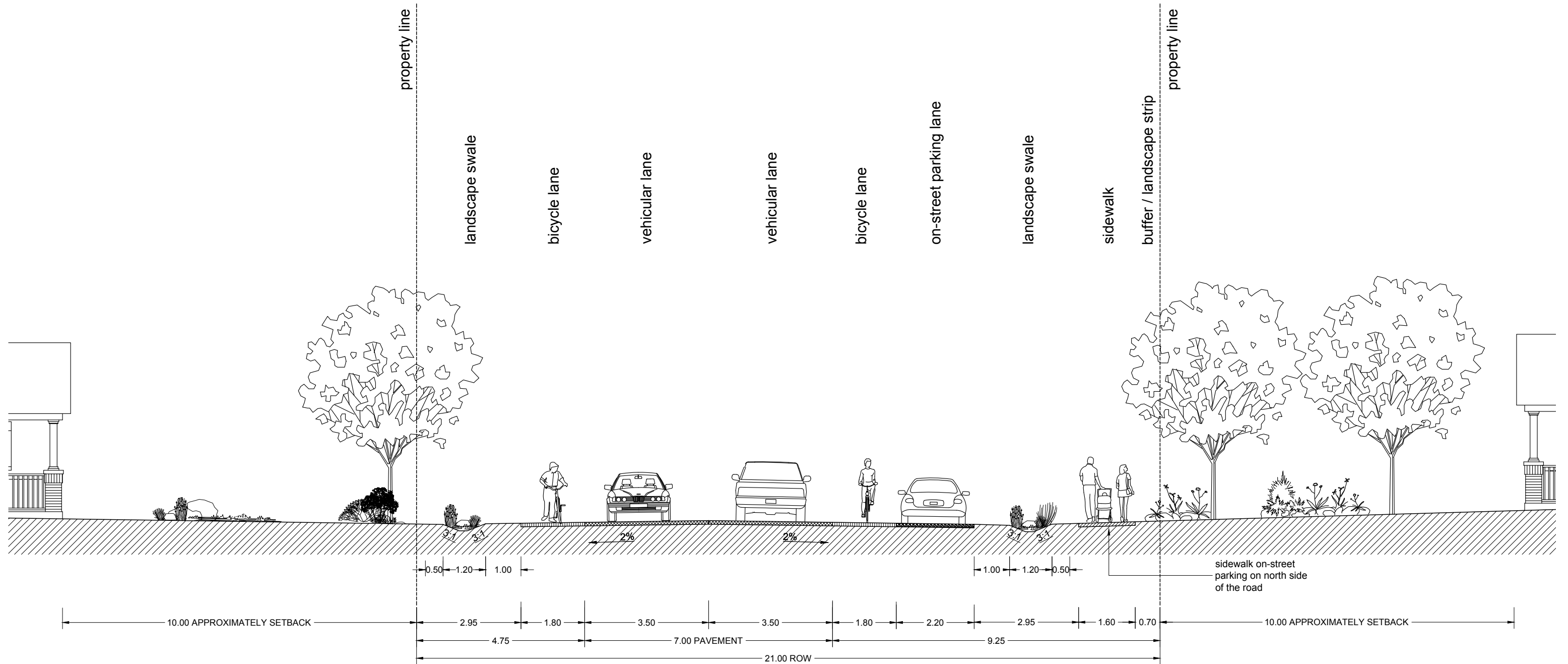
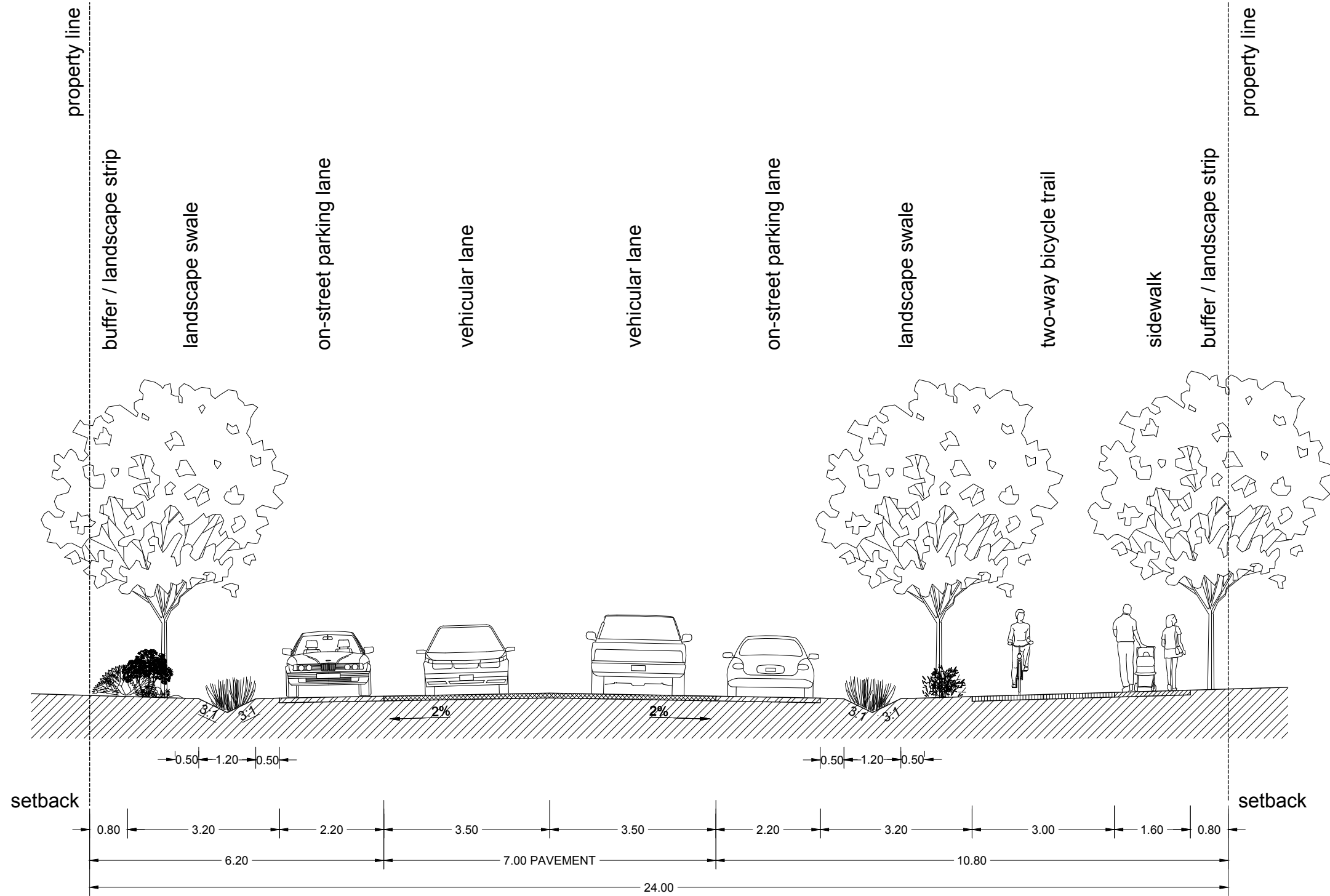


Figure 5. Canboro Road - Option 3





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b) Cream Street

Cream Street, located on the east boundary of the **East Fenwick Secondary Plan** area is designated as a Local road in the **Official Plan**. Cream Street's combination of mature vegetation and rolling hills make for an scenic drive, especially at the north end at Memorial Drive. Cream Street has a consistent right of way of 20.0 metres. The proposed streetscape is composed of:

- A total right of way width of 20.0 metres;
- Two vehicular traveling lanes;
- On-street cycling lane on both sides of the street;
- A pedestrian sidewalk on the west side of the street; and,
- Landscaped swale/boulevards with no street curb on both sides of the road.

c) Balfour Street

Balfour Street, located on the west boundary of the **East Fenwick Secondary Plan** area is designated as a Local road in the **Official Plan**. Balfour Street has a consistent right of way of 20.0 metres. The proposed streetscape is composed of:

- A total right of way width of 22.0 metres;
- Two vehicular traveling lanes;
- On-street cycling lane on both sides of the street;
- On-street parking on the east side of the road;
- A pedestrian sidewalk on the east side of the street; and,
- Landscaped swale/boulevards with no street curb on both sides of the road.

d) Memorial Drive

Memorial Drive, located on the north boundary of the **East Fenwick Secondary Plan** area is designated as a Local road in the **Official Plan**. Memorial Drive has a consistent right of way of 20.0 metres. The proposed streetscape is composed of:

- A total right of way width of 20.0 metres;;
- Two vehicular traveling lanes;
- On-street cycling lane on both sides of the street;
- A pedestrian sidewalk on the south side of the street; and,
- Landscaped swale/boulevards with no street curb on both sides of the road.



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Figure 6. Cream Street and Memorial Drive – Option 1

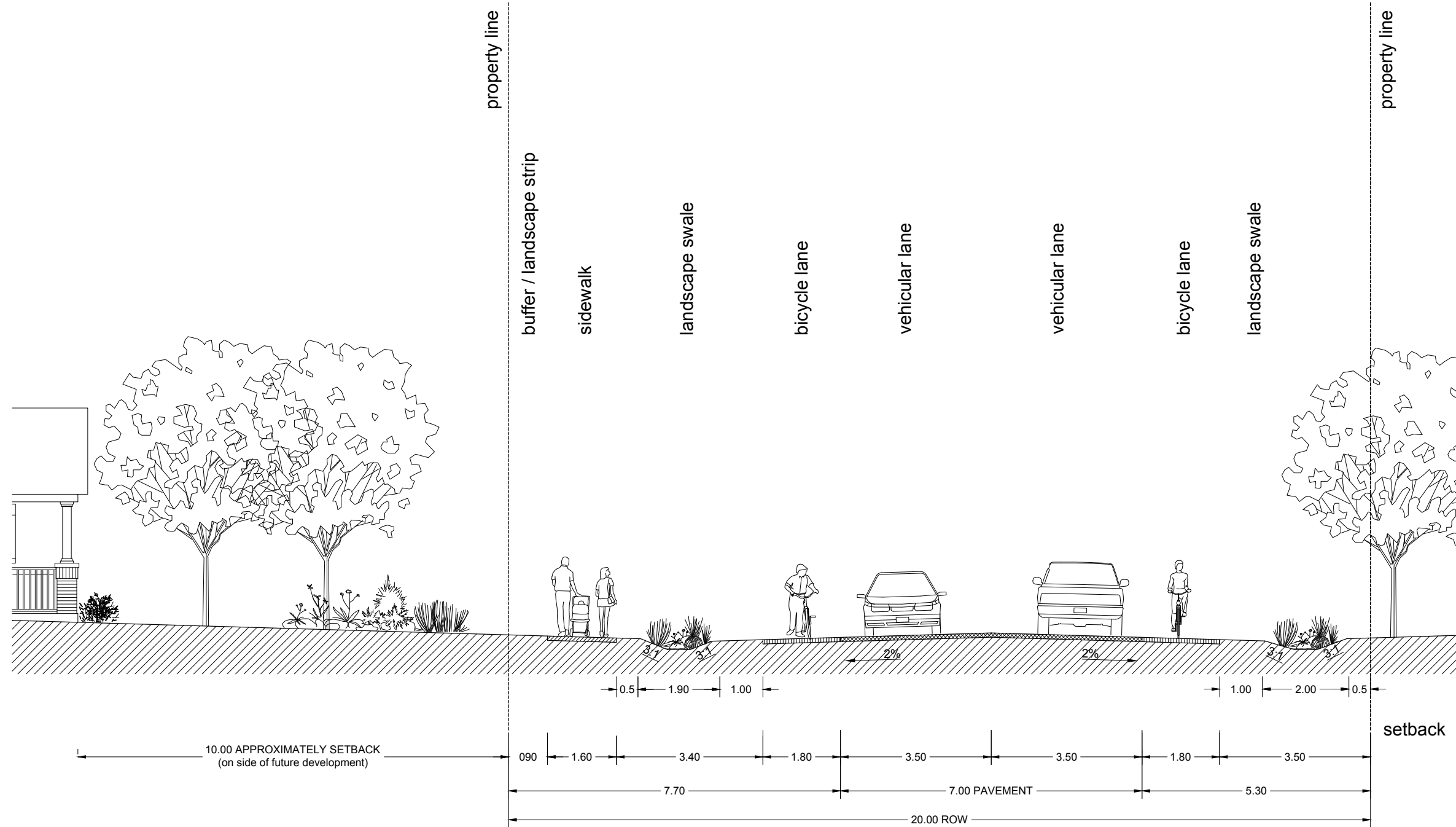
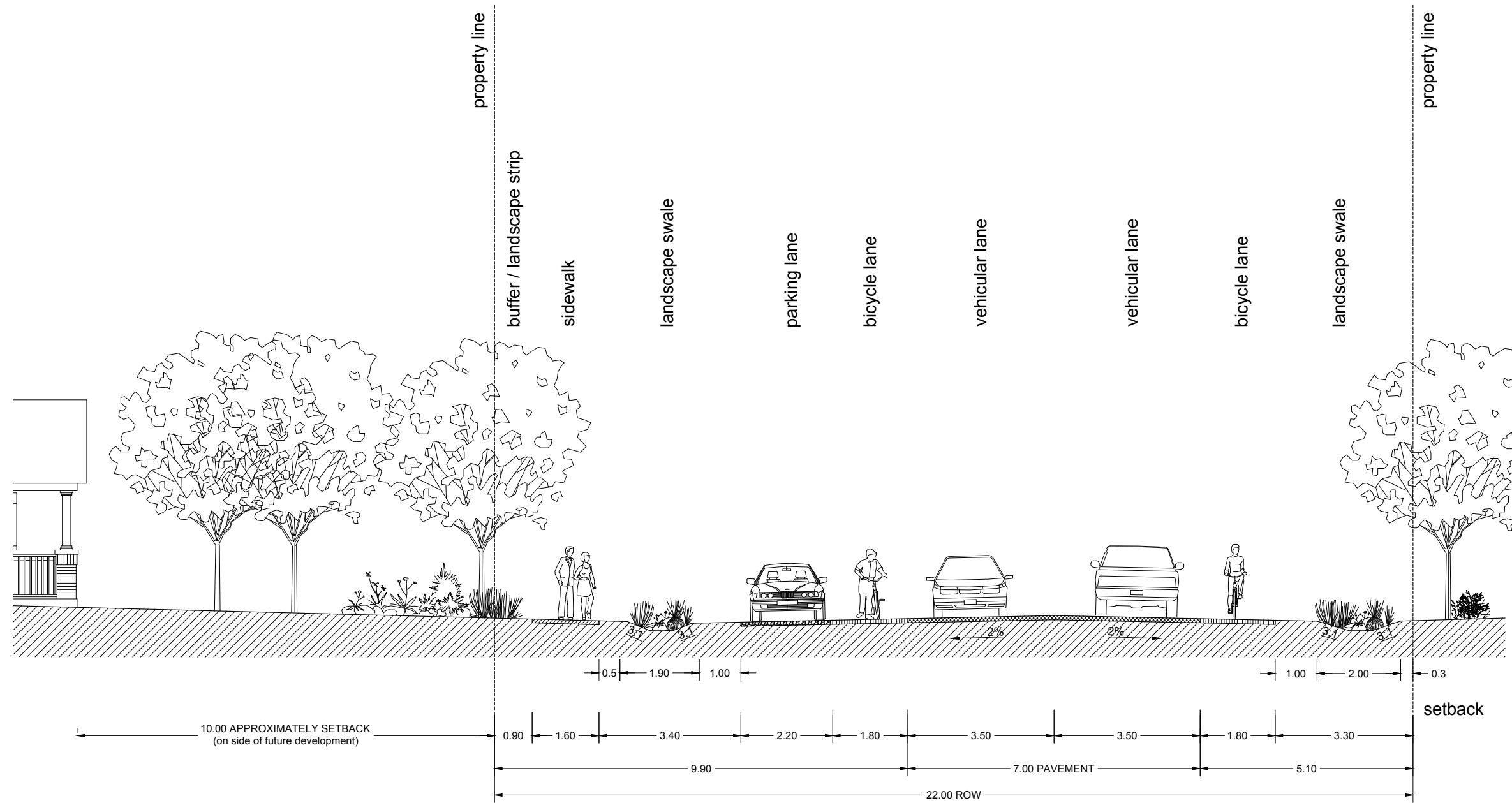


Figure 7. Balfour Street – Option 1



e) Welland Road

Welland Road, located on the south side of the **East Fenwick Secondary Plan** area is designated as a Collector road in the **Official Plan**. Welland Road has a consistent right of way of 20.0 metres. Two street cross Sections have been designed for the road to include:

Option 1

- A total right of way width of 20.2 metres;
- Two vehicular traveling lanes;
- On-street cycling lane on both sides of the street;
- A pedestrian sidewalk on the north side of the street; and,
- Landscaped swale/boulevards with no street curb on both sides of the road.

Option 2

- A total right of way width of 24.0 metres;
- Two vehicular traveling lanes;
- On-street cycling lane on both sides of the road;
- On-street parking on both sides of the road;
- A pedestrian sidewalk on the north side of the road; and,
- Landscaped swale/boulevards with no street curb on both sides of the road.



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Figure 8. Welland Road – Option 1

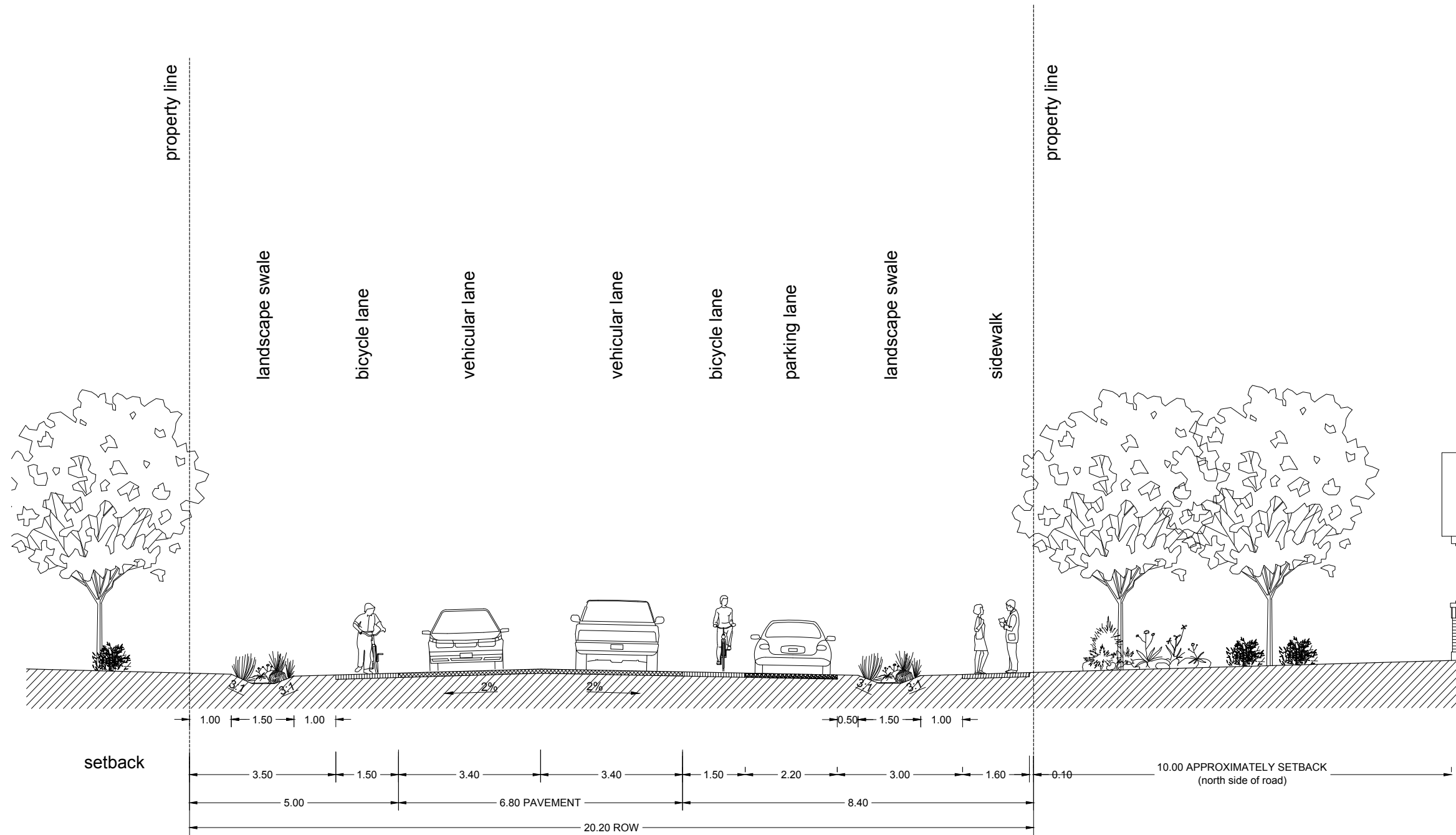
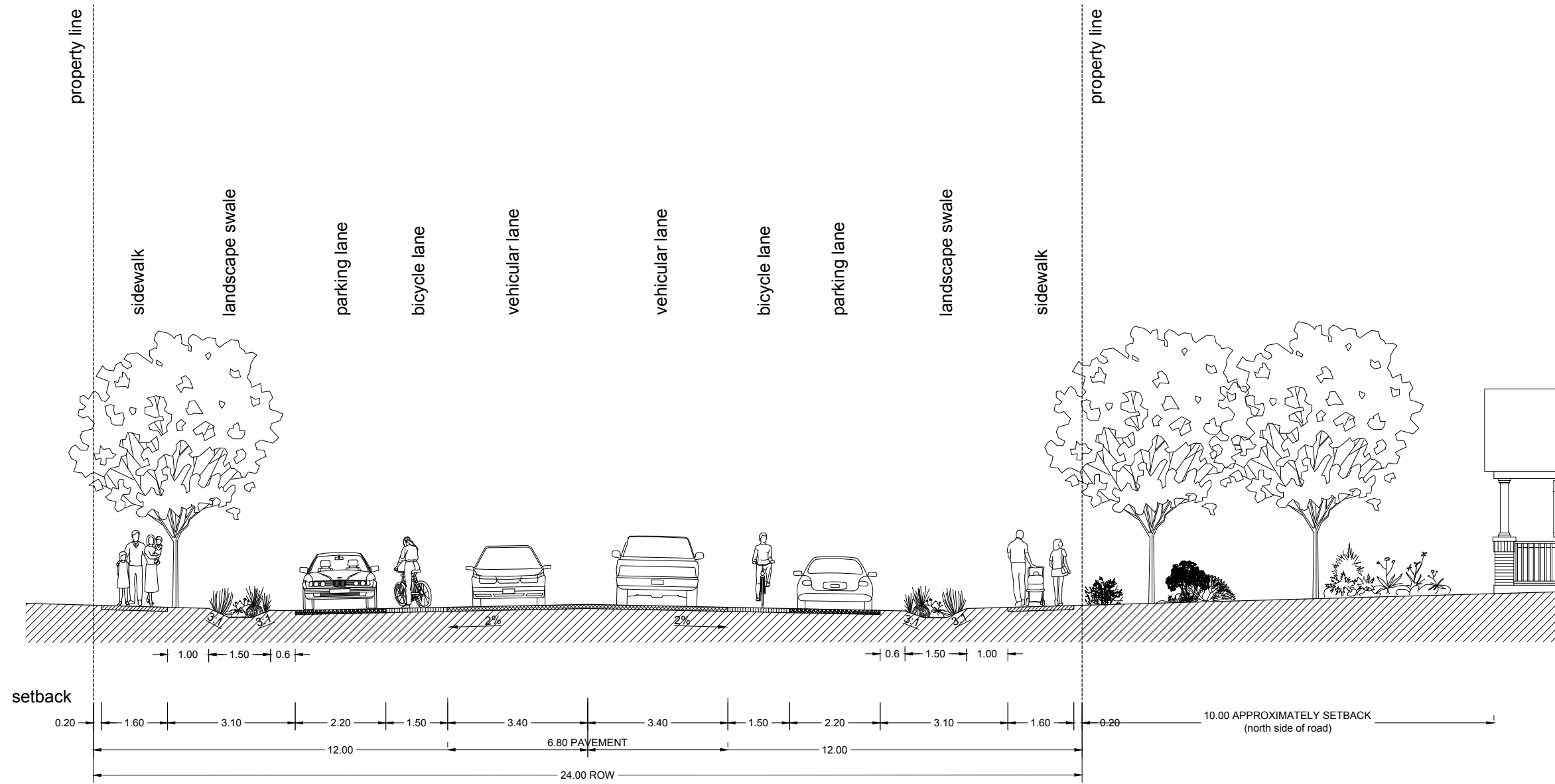


Figure 9. Welland Road - Option 2



4.1.2.2. New Street Design

The following Section describes the proposed design standards for new road topology within the East Fenwick study area.

a) Minor Collector Road

The community's future Minor Collector Roads are envisioned as pedestrian, cycling, transit and vehicular roads, wider than local streets and easily identifiable as the community's main through roads. Two Minor Collector Road cross sections have been developed to address site specific conditions.

The inclusion of bump out boulevards at key intersections is encouraged to support a comfortable pedestrian experience. Bump outs should be the same width from the curb as on street parking.

Option 1 allows for the incorporation of site specific drainage features into the overall streetscape design by realigning the site's existing drainage feature in a median. The proposed streetscape is comprised of:

- A total right of way width of 34.0 metres;
- A landscaped median with a bio-swale;
- Two vehicular traveling lanes;
- Off-street parking on both sides of the street;
- Landscaped swale/boulevards with no street curb on both sides of the road;
- On-street cycling lane on both sides of the street; and,
- A pedestrian sidewalk on the both sides of the street.

Option 2 streetscape is comprised of:

- A total right of way width of 22.0 metres;
- Two vehicular traveling lanes;
- On-street cycling lane on both sides of the street;
- On-street parking on both sides of the street;
- Landscaped swale/boulevards with no street curb on both sides of the road; and,
- A pedestrian sidewalk on the both sides of the street.



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Figure 10. Minor Collector Road - Option 1

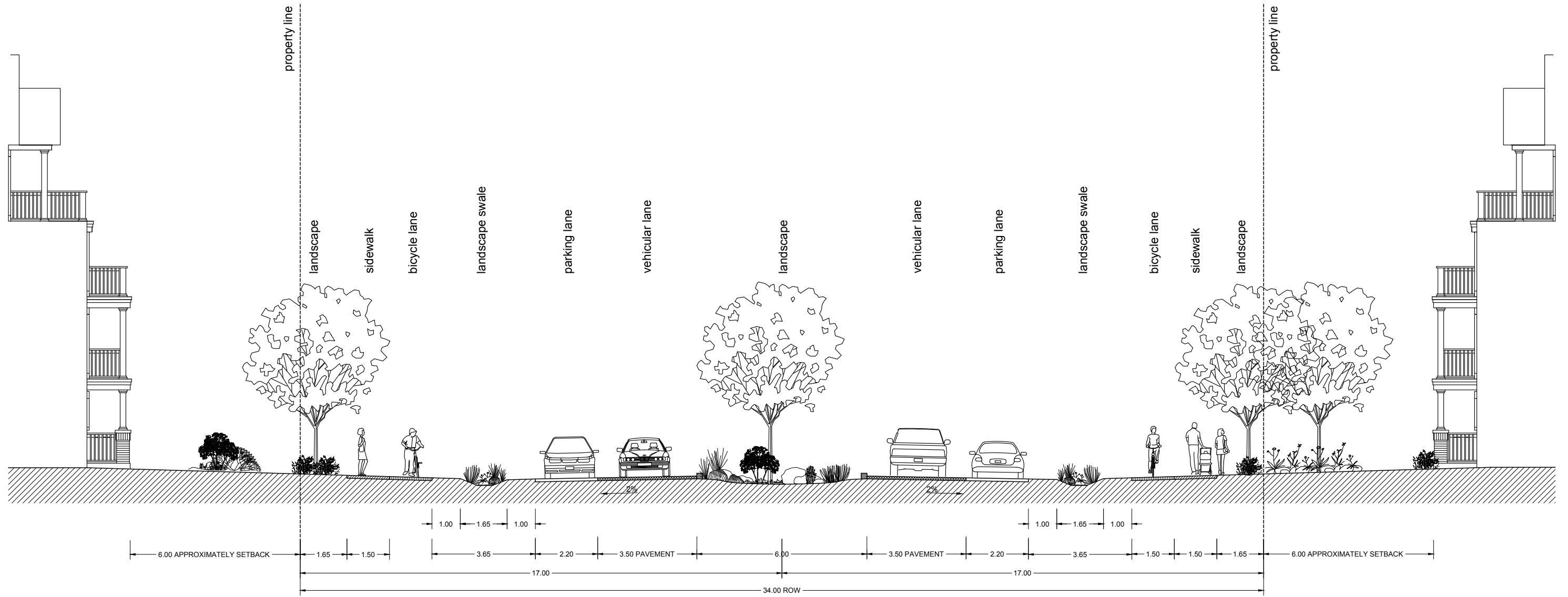
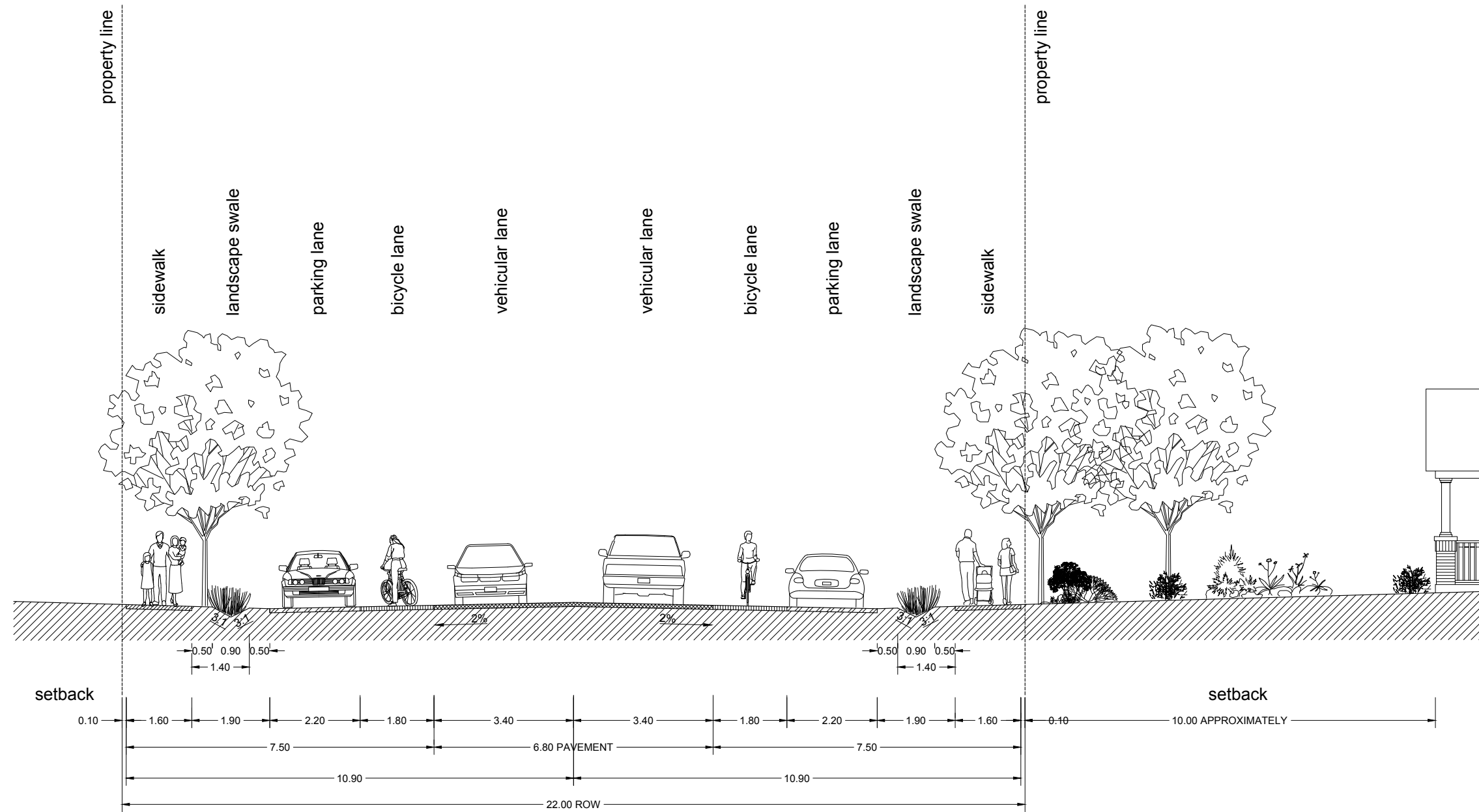


Figure 11. Minor Collector Road - Option 2



b) Local Street

Local streets are intended to be pedestrian friendly, "share the road" cycling routes. Their proposed streetscape is comprised of:

- A total right of way width of 16.0 metres;
- Two vehicular traveling lanes;
- On-street cycling: shared the road program;
- On-street parking on one side of the street;
- Landscaped swale/boulevards with no street curb on both sides of the road; and,
- A pedestrian sidewalk on the both sides of the street.

c) Laneway

Rear laneways help to create beautiful streets because parking driveways and service areas can be located along them while permitting buildings to front with living spaces and porches onto the street. The following are guidelines for laneway design:

- Rear laneways are proposed to be used in specific conditions where direct fronting onto existing hedgerows and treed features is desired.
- A 7.5 metre lane is proposed for the **East Fenwick Secondary Plan** area as illustrated in the following cross section.



Image 41. Seattle - Distinctive Materials for Pedestrian Crossing and Signage



Figure 12. Local Road

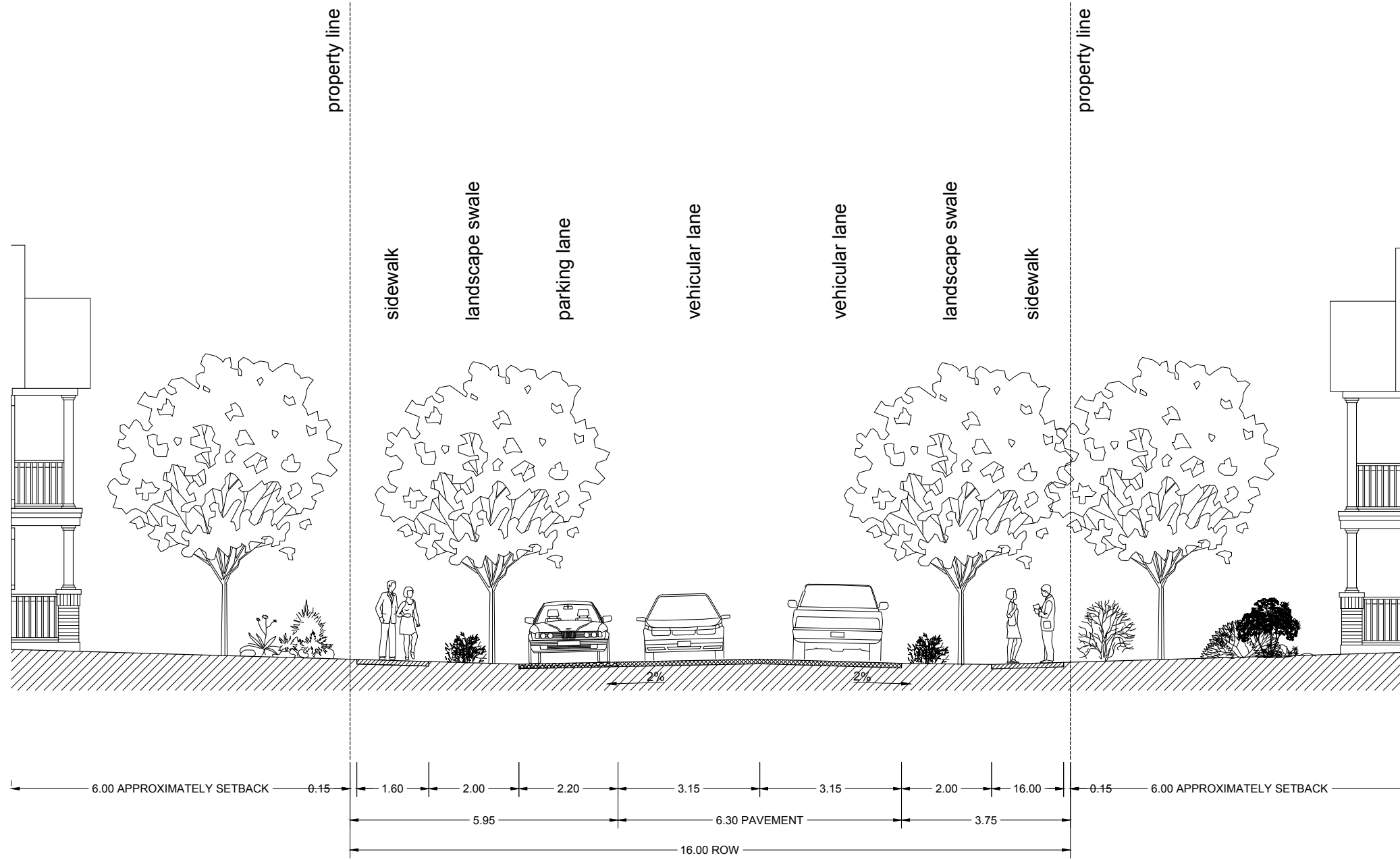
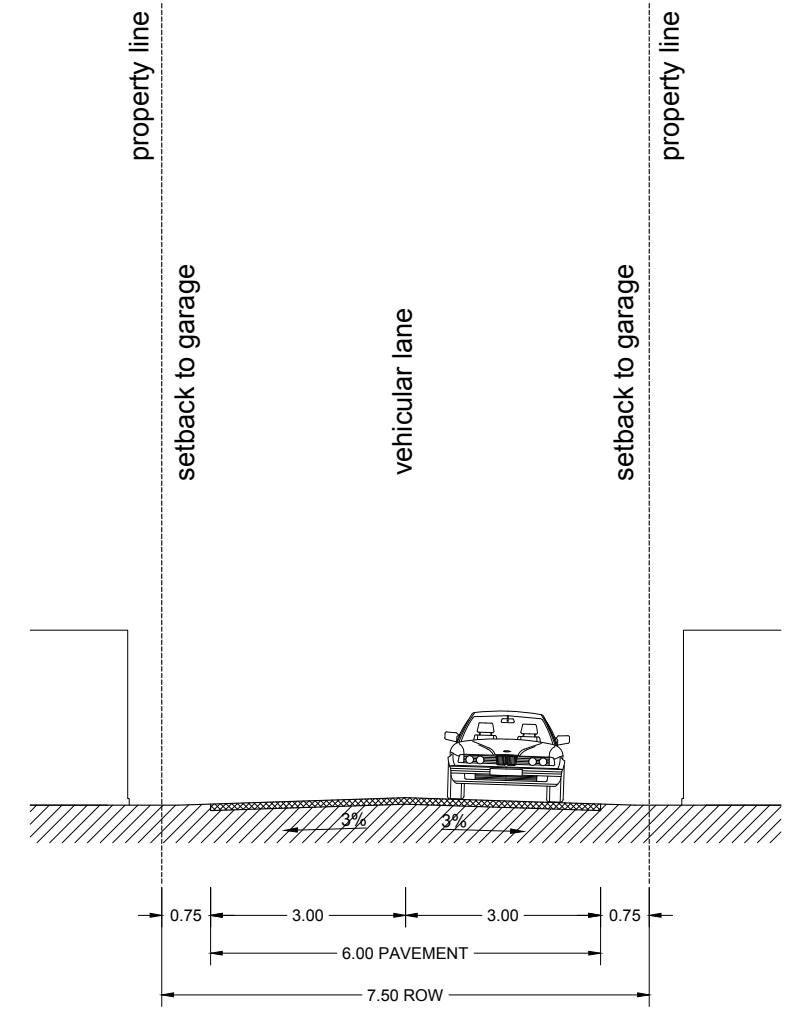


Figure 13. Laneway





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4.1.3. STREETScape ELEMENTS

a) Street Trees

1. For new streets, deciduous street trees should be provided on both sides of the road in the public right-of-way. At least 1 street tree should be planted for each residential dwelling unit (excluding multiple dwellings that are subject to site plan approval), or at an interval of 6.0 to 9.0 metres, and at least 3 street trees for each linkage lot where practicable based on factors such as utility requirements, driveway and street furniture locations and the type of species. Where it is not possible to provide the target number of trees as set out above, an equivalent number of trees should be provided in other locations within the draft plan of subdivision.
2. When selecting street tree species, trees that are compatible with bioswales and road salt should be selected. Where it may not be feasible to have both street trees and bioswales, it is encouraged that at least one tree be planted on the private front yard of dwellings that are adjacent to the sidewalk.
3. Street trees should be located at a minimum 1.0 metre away from a driveway edge, although 2.0 metres is preferred.
4. Street trees should be of a species that would provide a large canopy and shade over sidewalks in order to reduce heat island effect and enhance pedestrian comfort and safety. Street trees should also allow for sun exposure in the winter.
5. A diversity of native tree species should be considered and delivered along each street, and a themed street tree planting should be encouraged to assist in identifying and enhancing certain areas within the neighbourhood. It is recommended that a mix of tree species be selected on the same street in order to prevent invasive species.



Image 42. Varied Street Tree Species along Memorial Dr.



Image 43. Fenwick, ON - Tree Planting on Private Front Yard of Dwellings Adjacent to Sidewalk



Image 44. The Kingsway, Toronto - Street Tree Canopy with Rural Cross Section

6. When selecting landscape treatments vernacular planting sightlines and colour should be considered.

b) Swales, enhanced Grass Swales and Bioswales

1. Swales typically require a large area and are therefore well-suited for installation within planting zones and medians in new street cross-sections. They consist of linear vegetated channels that convey, treat and attenuate storm water runoff. Vegetation and check dams may be integrated into swales to slow velocity of runoff, allowing for sedimentation, filtration, evapotranspiration and infiltration.
1. Enhanced grass swales are similar to traditional grass swales, however they feature a slightly altered parabolic form and incorporate amended soils that slow runoff and assist in contamination removal. Enhanced grass swales might be used in particularly low traffic volume roadways that do not receive high loads of de-icing compounds in the winter.
1. Bioswales have a filter media and/or storage gallery and optimal under drain (depending on native soil permeability) below. Bioswales might be planted with grasses or finished with more elaborate combinations of plant and aggregate materials.

c) Street Furniture

1. Street furniture should be provided in high-pedestrian areas, such as parkettes. Furnishings should include elements such as pedestrian scaled lighting, benches, bicycle racks, newspaper boxes, and three stream receptacle bins. Where possible street furniture should be manufactured from recycled materials.
2. The Town may develop, in consultation with landowners, a unified standard and design vocabulary for street furniture, which shall apply to all new East Fenwick neighbourhoods. This design may be a continuation from the Fenwick Downtown Revitalization Project.



Image 45. UBC Rain Garden



Image 46. Portland's "Green Street" project



Image 47. Planted Bioswale

d) Pedestrian Crossings

Pedestrian crosswalks generally serve two functions:

- Separate the pedestrian realm from vehicle lanes by demarcating a safe route for pedestrians to cross the street; and
- Act as traffic calming measures since frequent stops will likely encourage slower traffic speeds and cautious driving.

Guidelines for pedestrian crossings are as follows:

1. Formal pedestrian crossings should be present at four-way intersections in high pedestrian areas, over all existing roads and the neighbourhood's future minor collector roads.
2. Pedestrian crossings should have a minimum width of 2.0 metres.
3. To enhance their visibility and quality, pedestrian crossings should utilize distinctive features such as alternative pavement markings or materials to minimize the conflict between vehicles and pedestrians. At a minimum, they should be identified with distinctive painted lines.
4. Pedestrian crossings should be highly visible to motorists and include appropriate signage.
5. Pedestrian crossings should be continuous and connected to adjacent sidewalks.
6. If pedestrian pathways are elevated, the height of curb cuts should be minimized to facilitate wheel-chair and stroller usage. Curb ramp designs at intersections should comply with all **AODA** standards and have raised tactile surfaces or materials with contrasting sound properties to help pedestrians with visual impairments.

e) Utilities

1. Utilities should be clustered and screened away from public streets and views.
2. High-efficiency light bulbs should be used, where feasible, for street lights and traffic lights as they consume significantly less energy, last longer and require less maintenance than conventional bulbs.
3. Pedestrian signalization should be automatic and not require a button to activate.

f) Utility Pole Placement and Design

1. Where feasible services should be co-located to reduce the number of poles installed for visual benefits and cost reduction. (A2.5.2 OP)
1. Poles should not interfere with historical site lines from key viewing areas such as squares, patios and other notable gathering spaces.
1. Poles should align with the corners of buildings and driveways, not the centre, to avoid blockages of existing or potential entrances, patios and windows (A2.4.1 OP).
1. Avoid visual clutter by aligning poles and/or new streetscape object with existing streetscape elements such as waste bins, benches and street trees to maintain existing sidewalk width and support a unified design vocabulary.

g) On-Street Parking

On-street parking plays a key role in the design of a sustainable community. While on-street parking accommodates visitor parking it also functions as a traffic calming device to slow traffic, and acts as a safety buffer separating the pedestrian realm from vehicles. The guidelines for on-street parking are as follows:

1. Parking should be provided on at least one side of the street for local and collector roads.
2. Parking bays should be located on collector and arterial roads where permanent parking is provided.
3. On-street parking areas may be demarcated with a special pavement treatment or with signage in order to distinguish the parking lane from the roadway or pedestrian and cycling pathways.



Image 48. Georgetown - Pedestrian Crossing



Image 49. City of Surrey - Painted Utility Box



Image 50. Brian Lancaster - Permeable, Demarcated Pavement Distinguishes Parking Lane

4.2. ACTIVE TRANSPORTATION INFRASTRUCTURE

Active transportation infrastructure includes walking, cycling and transit networks and infrastructure, which allow for a fully integrated pedestrian-supportive urban system. In addition to the policy direction in the *Secondary Plan* and the *Town's Official Plan*, the following guidelines apply.

4.2.1. PUBLIC PEDESTRIAN WALKWAY NETWORK

Official Plan policy A2.3 encourages new neighbourhoods to be compact and pedestrian friendly. Pedestrian walkways are an important design element that creates flexibility, giving pedestrians options to get to their destination using the most direct route. Public pedestrian walkways are located within the public realm. In order to encourage walkability within the neighbourhood, and improve circulation and pedestrian access, street-related pedestrian walkway shall be required within the neighbourhood. The following are guidelines for creating accessible, safe and inviting pedestrian walkways:

1. Public pedestrian walkway blocks should be 1.5 to 1.6 metres wide.
 - Adequate fencing and fenestration adjacent to public walkways to provide opportunities for casual surveillance; and,
 - Integrating activity generating uses such as seating areas, parkettes or other amenities.
2. As per *Official Plan* policy B1.6.4.6, roads shall be developed to include safe, convenient and attractive facilities for pedestrians. Crime Prevention Through Environmental Design (CPTED) principles should be used in the design of public pedestrian walkways, such as:
 - Adequate lighting;
 - Clear sight lines, allowing view from one end of the walkway to the other;
 - Provide appropriate landscaping, but avoid landscaping that might create blind spots or hiding places;
3. Where feasible, sidewalks should be continuous throughout the community.
4. In order to accommodate the needs of persons with disabilities, and the elderly, sidewalks should be designed to applicable municipal and *AODA* standards.

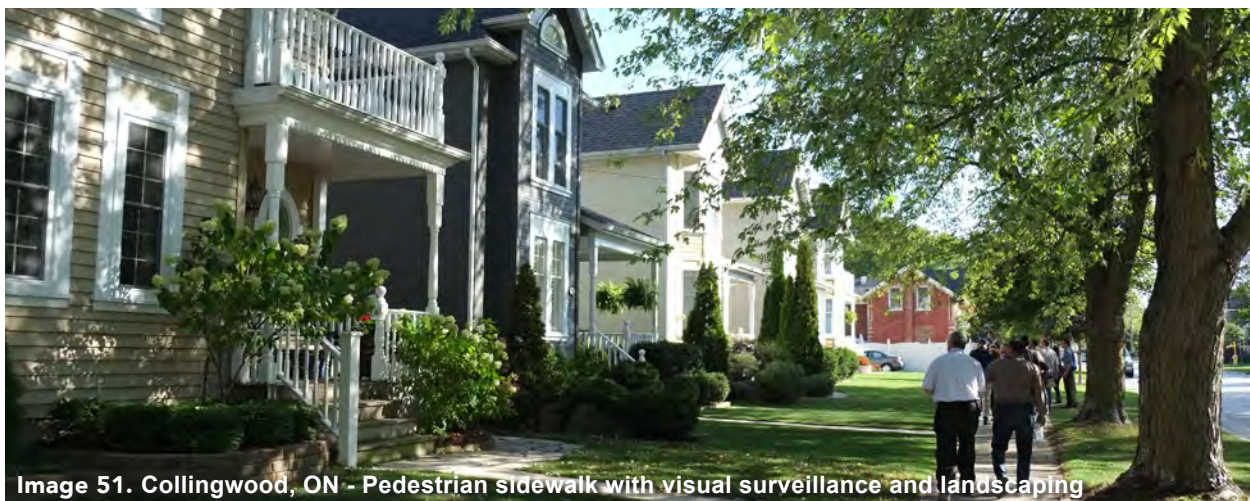


Image 51. Collingwood, ON - Pedestrian sidewalk with visual surveillance and landscaping

4.2.2. CYCLING NETWORK

On-street and separated bicycle lanes have been proposed for the **Secondary Plan** area. Where possible, new bicycle lanes will link with the existing cycling network within Fenwick and the Town of Pelham, as per the **Town's Official Plan** and the Niagara Region's Cycling Network. The following are guidelines for the cycling network:

1. Where cycling routes intersect major intersections, route signage should be provided.
2. Access to cycling routes should be clear and unobstructed.
3. Pedestrian and cycling lanes should be painted along multi-use trails or clearly identified by other means to minimize pedestrian and cycling conflicts.



Image 49. Share the Road - Beacon Lite (left), Shared Pathway - Barricade Traffic Services Inc (right)



4.2.3. TRANSIT NETWORK

The following are guidelines pertaining to transit facilities and infrastructure.

1. Future transit stops should be located in accessible, safe, and comfortable locations, which maximize pedestrian accessibility from surrounding residential neighbourhoods. In addition such transit stops should be:
 - Located close to intersections;
 - Coordinated with neighbourhood trail connections and building entrances; and
 - Designed to include amenities for passengers such as seating and weather protection.
2. Transit stops should generally be within 400 metres or a 5-minute walking distance for residents.

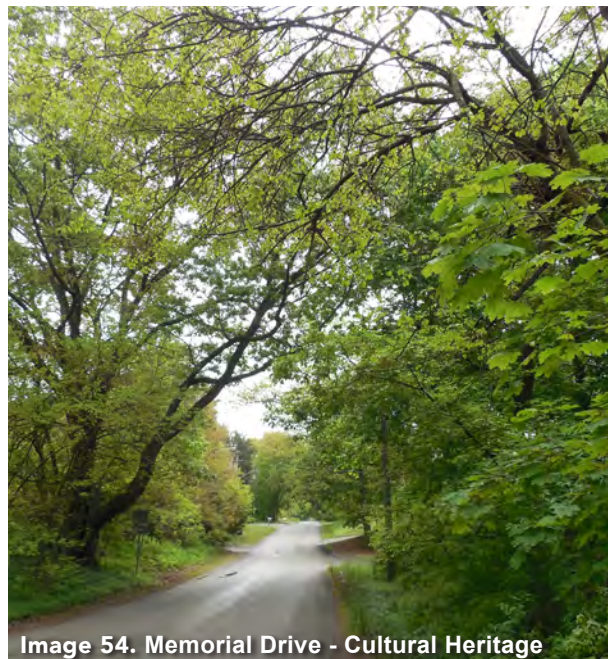


Image 52. Town of Pelham Bus Stop

4.3. NATURAL HERITAGE SYSTEM

A Natural Heritage System (NHS) has been established, which is comprised of a Provincially Significant Wetland and a Significant Wildlife Habitat area. The designated area is subject to further Environmental Impact Studies that must be reviewed and approved by the Region in consultation with the Town and the NPCA. The following guidelines are for land designated in the NHS:

1. Integrate the NHS as a key structural element of the neighbourhood by providing appropriate views, vistas and connections, as well as providing a range of development interfaces that offer opportunities for public visual and physical access while also limiting access where necessary.
2. Physical barriers, such as lot fencing, should be reviewed and considered in areas where access and encroachment are to be restricted. Fencing should be coordinated as a condition of draft plan approval to ensure consistent fencing design is established.
3. For homes adjacent to the NHS, homeowner education and stewardship should be encouraged through the distribution of a homeowners pamphlet as a condition of draft approval. Information in the pamphlet may include conservation practices and protection of the surrounding NHS, which include the identification of native plant species appropriate to the existing ecosystem, and the avoidance of organic waste dumping.
4. Carefully consider the design and location of parking areas, three stream receptacle bins or street furniture to ensure that the edge of the NHS is not degraded. This can be achieved through larger setbacks or landscaped buffers with private and decorative fencing.



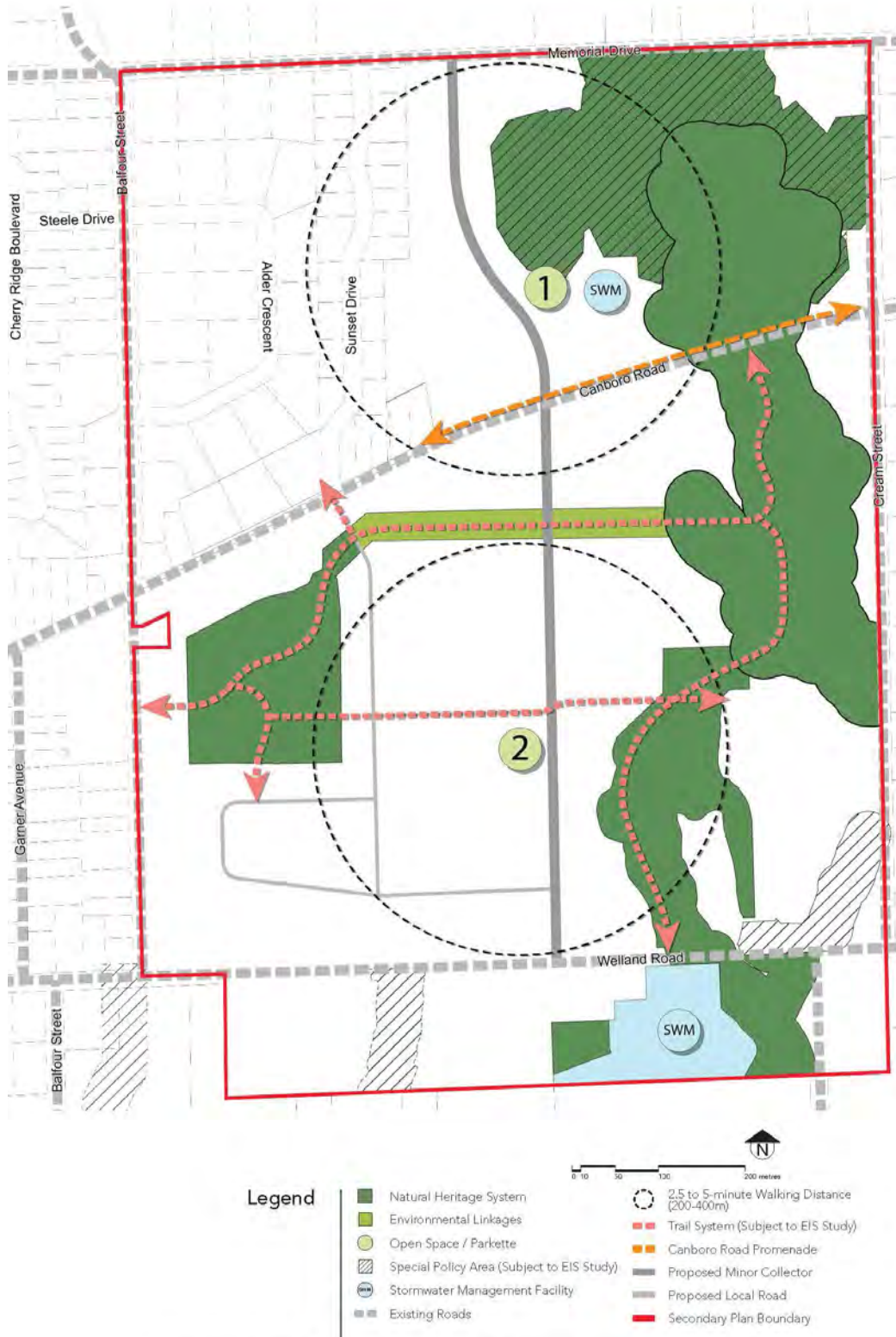
4.4. OPEN SPACE NETWORK

4.4.1. GENERAL GUIDELINES

The *East Fenwick Secondary Plan* area is comprised of three interconnected neighbourhood parkettes and an extensive Natural Heritage System. The following are general guidelines for the open space or greenlands network.

1. New trees and landscaping within parks should be of native plant materials, and where possible, should be salvaged from the site or the local area.
2. Amenity areas within parks should be located and oriented to maximize sunlight and be sheltered from the wind, noise, and traffic of adjacent streets and uses. Amenities can include playground or outdoor fitness equipment.
3. Seating and shade areas should be designed in coordination with pathways and play area locations.
4. Parkettes should be designed and located to utilize Crime Prevention Through Environmental Design (CPTED) principles by ensuring clear view into and out of surrounding areas, which include:
 - Adequate lighting;
 - Fronting buildings to overlook public spaces, especially for playgrounds, which should be highly visible to public streets and/or houses, in order to enhance safety.
 - Using signs and design for ease of access and egress; and
 - Mix of activity for constant use of the space.
5. Playground amenities are encouraged to be designed as learning amenities constructed from natural materials.
6. Parks should include appropriate signage visible from surrounding streets.
7. Wayfinding signage should be designed with a unified design vocabulary for ease of use and to further build a unique East Fenwick neighbourhood identity.

Figure 14. East Fenwick Secondary Plan - Greenlands Structure Plan



Schedule A8. East Fenwick Secondary Plan Area
Greenlands Structure Plan

4.4.2. **PARKETTES**

As per the *Town's Official Plan* policy B1.7.9.4.1, parkettes shall be designed to reinforce a high quality formalized relationship with its adjacent building use, streetscape and/or component of the Green Lands system. Landscape elements and features within the parkette shall be designed to define and articulate activity areas, circulation, entry points, seating and gathering areas. In addition to these policies, the following guidelines apply;

1. A parkette should provide soft landscapes to serve non-programmed recreational uses.
2. Parkettes should accommodate facilities such as play structures, splash pads, open informal play areas, seating with shade opportunities as well as community information boards.
3. Public frontage is encouraged on all four sides of the parkette. This can be in the form of roads or houses fronting onto the parkette with the use of rear lanes.
4. Residential units on roads abutting the parkette should front onto the parkette.
5. Landscape treatment and built form elements should be located at entries to and/or at view corridors terminating at parkettes in order to create a terminus/landmark in the community.
6. Formalized paths within parkettes should connect to pedestrian walkways and trails within the NHS, and should be consistent with pedestrian desire lines.
7. Where residential side yards abut a parkette, a combination of decorative fencing, landscaping, and privacy fencing should be provided to demarcate the public and private realm, and to ensure the privacy of the residence is maintained.



Image 55. Cornell - Parkette within woodlot, with soft landscaping, play structure and pathways

4.4.3. PEDESTRIAN SYSTEM & TRAIL NETWORK

As per the *Town's Official Plan*, continuous and connected open space and trail systems are to be developed that link the local neighbourhood to the broader town and encourage active transportation. The *Secondary Plan* has proposed an East Fenwick network of open spaces, that are comprised of the existing NHS along with a series of small neighbourhood parkettes that are interconnected with a pedestrian system or trail network. The following are additional guidelines for the development of pedestrian systems and trail network:

1. A primary neighbourhood trail system will function as a north-south and east-west spine that links adjacent neighbourhoods. The system may traverse through local streets or through the NHS with a width ranging from 2.0 to 3.0 metres. Recreational trails within the NHS will function as multi-use trails that support both destination and recreational use and shall generally consist of a 2.4 metre wide trail.
2. Native non-invasive plantings are encouraged along trail connections abutting natural features.
3. Trails will be designed to accommodate a range of users and abilities, and should be barrier-free where appropriate. The use of permeable materials should be encouraged for trail construction in areas where sufficient drainage exists.
4. Trails should be clearly signed regarding permitted uses and speed. Wayfinding signage and/or trail markers should have a unified design vocabulary and be provided throughout the trail network.
5. Multi-use designation symbols should be placed at the beginning of a trail, the entrance to any stage of a crossing, and shortly after a crossing. They can also be placed at intervals along the length of a trail to remind trail users to keep to the right. Directional arrows can also be used in conjunction with designation symbols to direct users. Designation symbols should be accompanied by signage outlining the permitted modes of travel on the trail.
6. Benches and three stream receptacle bins should be provided at trail heads and at regular intervals along the route.
7. Trails located in proximity to significant sensitive natural features or adjacent to stormwater management facilities should incorporate interpretive signage at various locations to promote stewardship initiatives that will protect and enhance the features and functions of the natural environment.
8. Special treatments at trail head entrances should be considered including high quality features such as landscaping, benches, interpretive or directional signage or wider pathway widths.

4.4.4. STORM WATER MANAGEMENT FACILITIES

The *East Fenwick Secondary Plan* has identified two stormwater management ponds, which are integrated with the open space system and aim to mitigate impacts on the natural environment. These stormwater management facilities shall be developed in a manner that will yield the greatest environmental and amenity benefits to the neighbourhood, which will first be achieved through reducing stormwater run-off and flow to the ponds, and secondly, through the design and landscaping of the pond.

Stormwater management facilities will be provided in accordance with the policies in Section C6 of the *Official Plan*. In general the overall design and grading of all proposed Stormwater Management Facilities should be in accordance with current Ministry of the Environment and Niagara Region requirements. This Section provides guidelines on pond design and landscaping, and should be read in conjunction with Section 5.2 Water Efficiency and Management for stormwater retention and run-off guidelines.

1. The stormwater management site will be a key focal/visual feature within the community, contributing to its appearance and ambiance. Therefore, careful consideration should be taken for the design and integration with the adjacent neighbourhood and open space.
2. The ponds should also be designed to blend with the natural landscape. Where feasible, inlet and outlet structures should be concealed using a combination of planting, grading and natural stone.



Image 56. Geo-scape Contracting - Stormwater Management Facility as Part of the Community's Integrated Active Transportation Network

3. Native species and flood-tolerant water's edge plants, including a mixture of herbaceous and woody vegetation, may be planted to stabilize banks of ponds. The perimeter of the permanent pool should be planted with emergent, strand and submergent species to improve the aesthetics and enhance the performance of the facility.
4. The density quality and amount of planting coverage should be considered when designing storm water management facilities.
5. Where there is a need to discourage public access to areas around the perimeter of the ponds, living fences and barrier plantings may be utilized in place of fencing. Barrier plantings should be installed along the crest of steep slopes, adjacent to deep-water areas and around inlet and outlet structures.
6. Fencing of ponds should be discouraged, except along rear or flankage residential property lines.
7. Pedestrian and cycling trails and viewpoints should be incorporated into the design of pond blocks, where feasible.
8. The Stormwater Management Facility Landscape Plan is to be prepared by a qualified Landscape Architect, reviewed by the Town's Public Works and Parks Departments, and approved by Town Council.
9. Walkways, trails, and other amenities such as fountains, benches, and rock walls may be incorporated into the facility design.
10. Paths shall not be located lower than the 100-year storm event elevation.
11. Side slopes adjacent to wet facilities must be a minimum of 5:1, (5 horizontal to 1 vertical). Slopes within the wet facility (between the permanent pool and the 100-year storm elevation) must be a minimum of 7:1. Slopes within the permanent pool must not exceed 3:1.
12. A minimum buffer of 5.0 meters shall be provided from 0.3 meters above the 100-year storm elevation to the property line.
13. The facility shall display an appropriately located sign that identifies the facility's purpose, warns the public of potential safety hazards, and includes a municipal phone number.



Image 57. Lichtschwärmer - Stormwater Management Pond with living fences, shallow slopes and pedestrian access

5. GREEN INFRASTRUCTURE & BUILDING

The guidelines in this section build on the Niagara Region's Smart Growth Design Criteria Completion Checklists and are listed as further guidance to receive funding incentives from the Region.

5.1. ENERGY EFFICIENCY

1. All new developments are encouraged to be built adhering to the following requirements for sustainability:
 - Grade related (3 storeys or less) residential buildings achieve a minimum performance level that is equal to ENERGY Star U standards; and,
 - Mid-rise (4 storeys and greater) residential buildings shall be designed to achieve greater energy efficiency than the Model National Energy Code for Building.
2. All buildings, with the exception of residential buildings containing less than 25 dwelling units shall achieve the following sustainability requirements:
 - LEED Silver, or equivalent, prior to and including 2021; and
 - LEED Gold or equivalent from 2022 up and beyond 2031.
3. Where feasible, alternative community energy systems such as geo-exchange, sewer heat recovery, and/or inter-seasonal thermal energy should be provided.
4. Grade related residential unit driveways are encouraged to be paved with light-coloured material to reduce the heat island effect.
5. Reflective or light-coloured roofs should be encouraged for multi-unit residential units above 5-storeys, and public or institutional buildings, in order to reduce solar heat absorption and energy demand.
6. Light-coloured, permeable material for all hardscape, including parking areas and pedestrian walkways, should be used for development with paved surface in order to reduce solar heat absorption and energy demand.
7. To minimize energy consumption and encourage the integration of passive building systems, buildings should be oriented to maximize the potential for sunlight and natural ventilation.
8. The strategic use of deciduous trees is strongly encouraged as part of a free cooling strategy to help with evapotranspiration and shading.
9. Building design is encouraged to utilize opportunities associated with large expanses of roof areas to implement solar thermal, photovoltaic systems, green roofs or water harvesting systems.
10. Energy Star compliant appliances are encouraged to be provided in cases where the developer provides appliances.
11. Alternative energy sources such as solar thermal, photo voltaic panels and/or geothermal technologies or the inclusion of "roughed in" facilities to accommodate such alternative energy sources are encouraged to be provided.
12. Charging stations for electric vehicles are encouraged in parking areas of apartments or condominiums.

5.2. WATER EFFICIENCY AND MANAGEMENT

1. All new buildings are encouraged to achieve a 20% or greater water conservation efficiency than that required by the Ontario Building Code.
2. Irrigation of all public open spaces/structures should implement a rainwater harvesting program, and can include the use of rain barrels, and/or infiltration trenches provided water balance objectives are met.
3. Water efficient landscaping for high-density blocks should use native and/or drought resistant planting to reduce the amount of watering needed.
4. Grade-related residential unit driveways should be paved with permeable materials to reduce stormwater run off.
5. At site-plan level, developments with five or more dwellings should be encouraged to increase the level of perviousness in order to promote at-source stormwater management, reduce peak flows and lessen the dependence on end-of-pipe facilities such as stormwater management ponds. Pervious areas can include landscaped areas and/or areas containing permeable paving.
6. In order to reduce the volume of run-off into the storm drainage system, surface water run-off flows should be directed to landscaped areas and the use of hard surfaces should be minimized.
7. Innovative sustainable technologies in the capture, conveyance, and treatment of storm run-off to reduce potential pollutants/contaminants are encouraged.
8. New residential units should be designed to incorporate gray water pipe infrastructure, where permitted, in accordance with the Ontario Building Code and health regulations.



Image 58. Natural Path Landscaping - Permeable, light pavement and light coloured roof

9. Indoor water usage should be minimized in new buildings through the installation and use of water-efficient fixtures, fittings and appliances, such as dual-flush toilets, faucets, and shower heads.
10. Low maintenance and drought-resistant landscaping is encouraged.
11. To conserve and protect the area's natural heritage features, stormwater runoff from all new developments shall be collected and treated to an Enhanced standard (as per MOE guidelines).
12. In order to avoid the negative impacts of flooding in the downstream watershed, all new development post development peak stormwater flows shall be attenuated to pre-development conditions for up to and including the 100 year return period design storm event.
13. All downspouts from residential development shall discharge to grade.
15. Adequate and safe minor and major storm conveyance systems shall be required for all new development.
16. Low Impact Development methods will be strongly encouraged to be utilized in this new community in conjunction with traditional stormwater management techniques.
17. The target is to capture, detain, utilize, or infiltrate all stormwater runoff from all storms up to the 25mm design storm event.
18. Increasing the size and capacity of the major and minor storm systems, and the associated stormwater management systems and facilities, should be considered in order to increase the community's level of resilience against the impacts of climate change (i.e. severe storm events).



Image 59. Philadelphia Water Department - Rainwater Barrel and Permeable Landscaping

5.3. MATERIAL RESOURCES & SOLID WASTE

1. A minimum of 25% recycled/reclaimed materials is encouraged to be used for new infrastructure including roadways, parking lots, sidewalks, unit paving, stormwater management facilities, sanitary sewers, and/or water pipes.
2. All buildings should utilize best practices for design and construction techniques in order to reduce the amount of construction waste produced.
3. Where feasible, streetscape and building materials should be chosen and determined by suitability, durability, ease of maintenance, and cost effectiveness.
4. Green building materials should be used to reduce impacts on the environment. Building materials should be purchased and/or obtained from responsible, ethical, and whenever possible, local sources.
5. The use of recycled and reclaimed material for new buildings is encouraged in order to reduce the negative environmental effects of extracting and processing materials.
6. Waste volumes should be reduced through the provision of recycling and/or re-use and compost stations as well as drop off points for potentially hazardous waste.



Image 57. LEED Categories - Gamma

5.4. LIGHTING

1. Lighting should be downcast to reduce light pollution and address night sky condition.
2. Exterior lighting spill-over onto the street or adjacent properties should be minimized.
3. Renewable energy sources should be considered for electric energy supply in the public realm (i.e. lighting for natural trails and park pathways).
4. High efficiency street lighting should be used so as to reduce energy consumption by at least 15% of the baseline annual energy use.
5. High efficiency lighting should be incorporated into the interior and/or exterior areas of a development, such as in common areas for multi-unit housing.
6. For residential buildings, external lighting should incorporate lighting controls that use motion sensors and/or timers to improve energy efficiency.



5.5. SUSTAINABLE PROGRAM

Sustainable programs should encourage opportunities for community-based initiatives such as car pooling, community composting, waste reduction, educational, and stewardship programs. Some examples of such programs are as follows:

a) Education Packages

1. Owner/tenant education packages regarding household activities to conserve household energy and water resources, access to transit, recycling and composting programs, and depots should be provided at the time of purchase or rental.
2. Encourage homeownership afford-ability for low-income families through the provision of sites for non-profit or charitable Homeownership Programs.

b) Mobility Programs

1. Carpooling or car share programs are encouraged for residents of high density residential buildings. Dedicated parking spaces for carpooling or car share programs should be provided, and located in close proximity to building entrances with clear signage.

2. The availability of transit programs should be publicized to new homeowners as part of an information package provided by developers.
3. Dedicated parking spaces for hybrid/fuel efficient or similar vehicles in high density/ multi-unit buildings should be provided, and located in close proximity to building entrances with clear signage.

c) Cycling Facilities

1. Multi-unit apartment or condominium buildings should provide residents with bicycle parking and/or storage that is secure, enclosed, and easily accessible to residents. Informational signage should be provided.
2. Secure, outdoor bicycle racks should be strategically located at destination points, including village greens and trail heads.



Image 62. Urban Racks - Outdoor Bicycle Racks



Image 63. My Ride - Pelham Transit Program



5.6. INNOVATIVE DESIGN

In order to encourage exemplary performance above the requirements set out in this document, innovative design is strongly encouraged and should be recognized.

Innovative elements and performance should, in writing, identify the intent of the proposed innovation, while demonstrating and describing the design approach and strategies utilized to achieve sustainable performance measures that exceed those set out in this document.

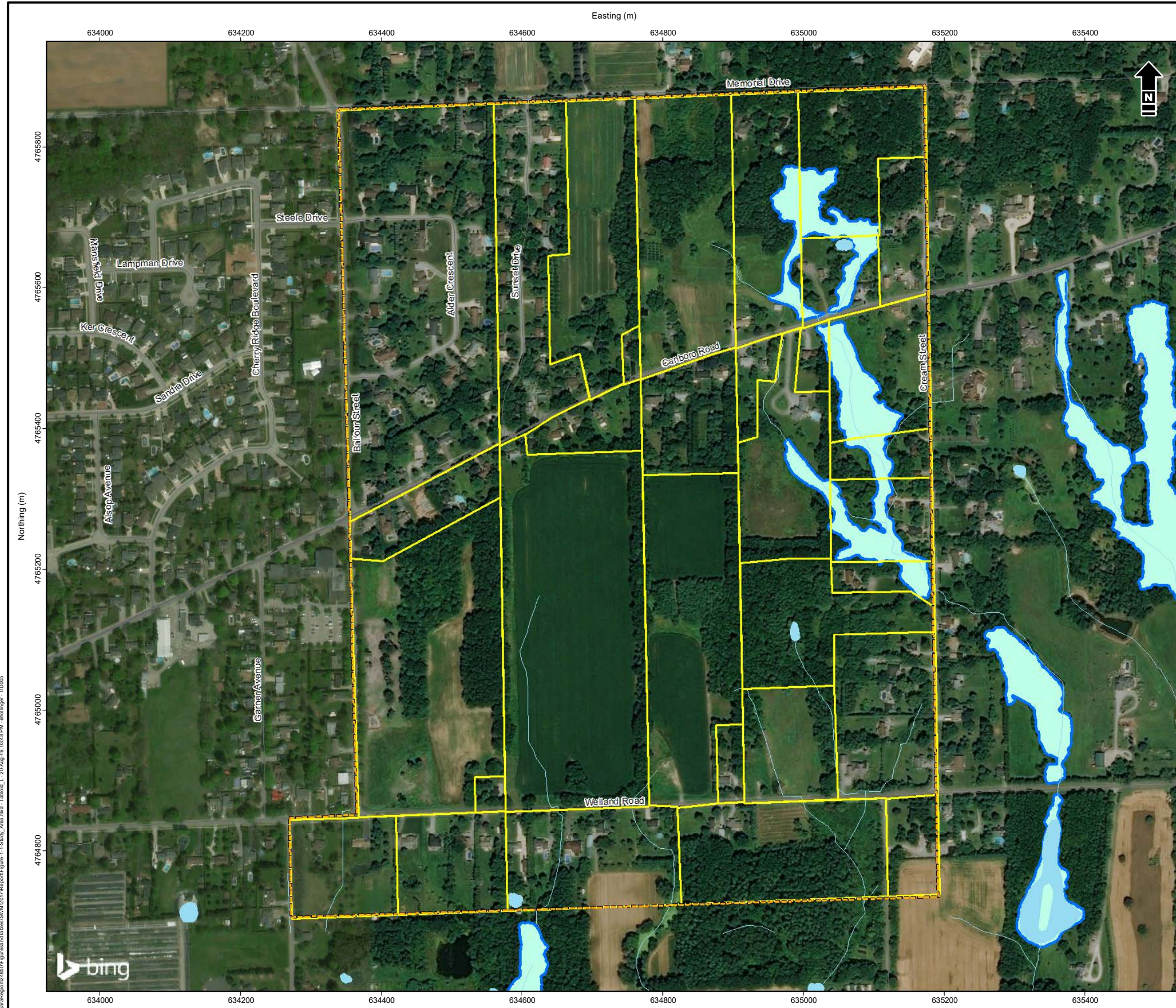


APPENDIX D - NATURAL HERITAGE SYSTEM

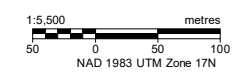




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- Study Area
- Property Boundary
- Provincially Significant Wetland
- Water Body
- Watercourse
- Highway
- Road



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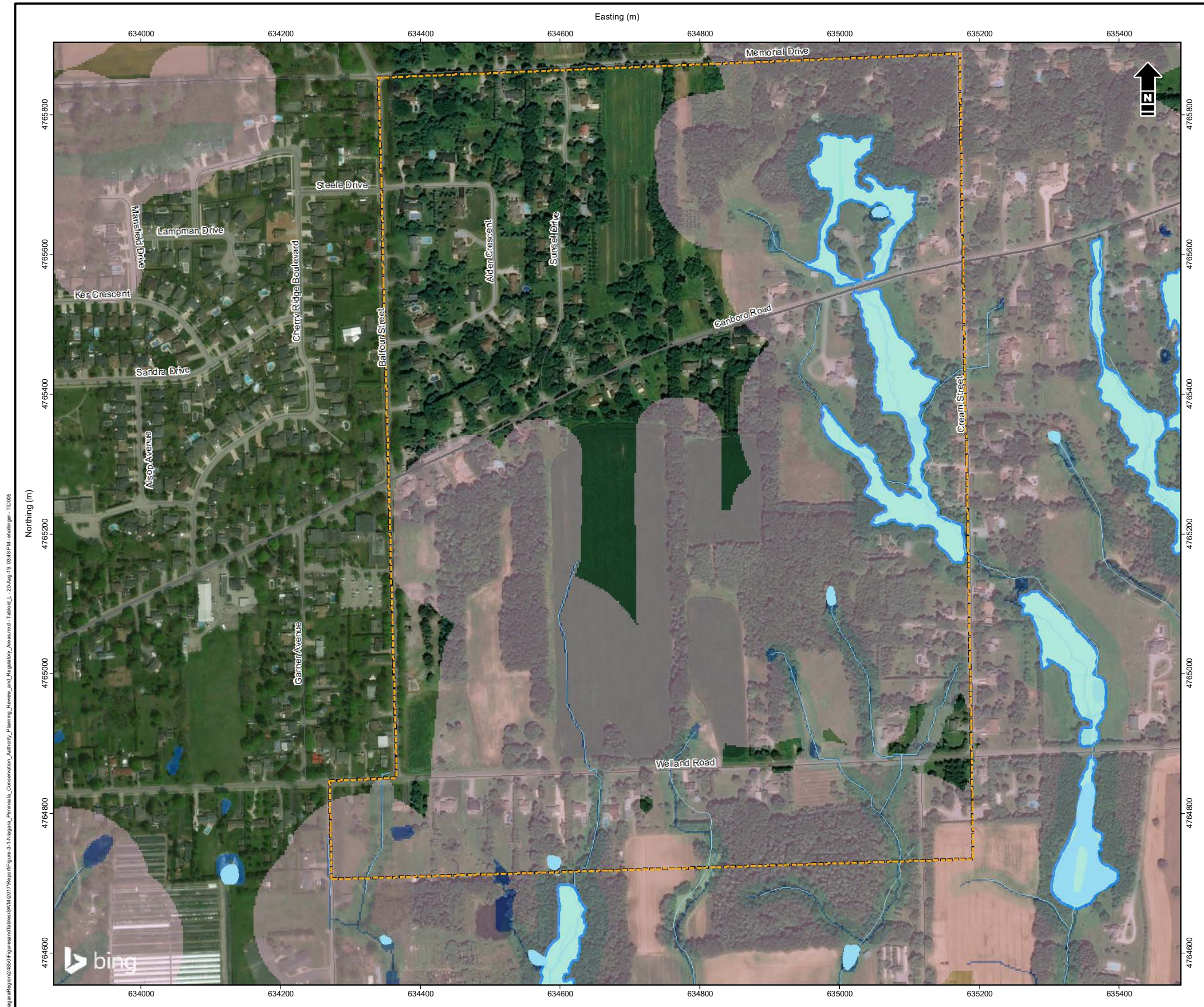


Niagara Region
East Fenwick Secondary Plan

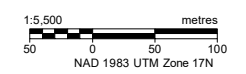
Study Area

Date: August 2019 Project: 24850 Submitter: E. Wilkinson Reviewer: S. Toner

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- Study Area
- NPCA Permit Review
- Regulated Wetland
- Planning Review Area
- Provincially Significant Wetland
- Water Body
- Watercourse
- Highway
- Road



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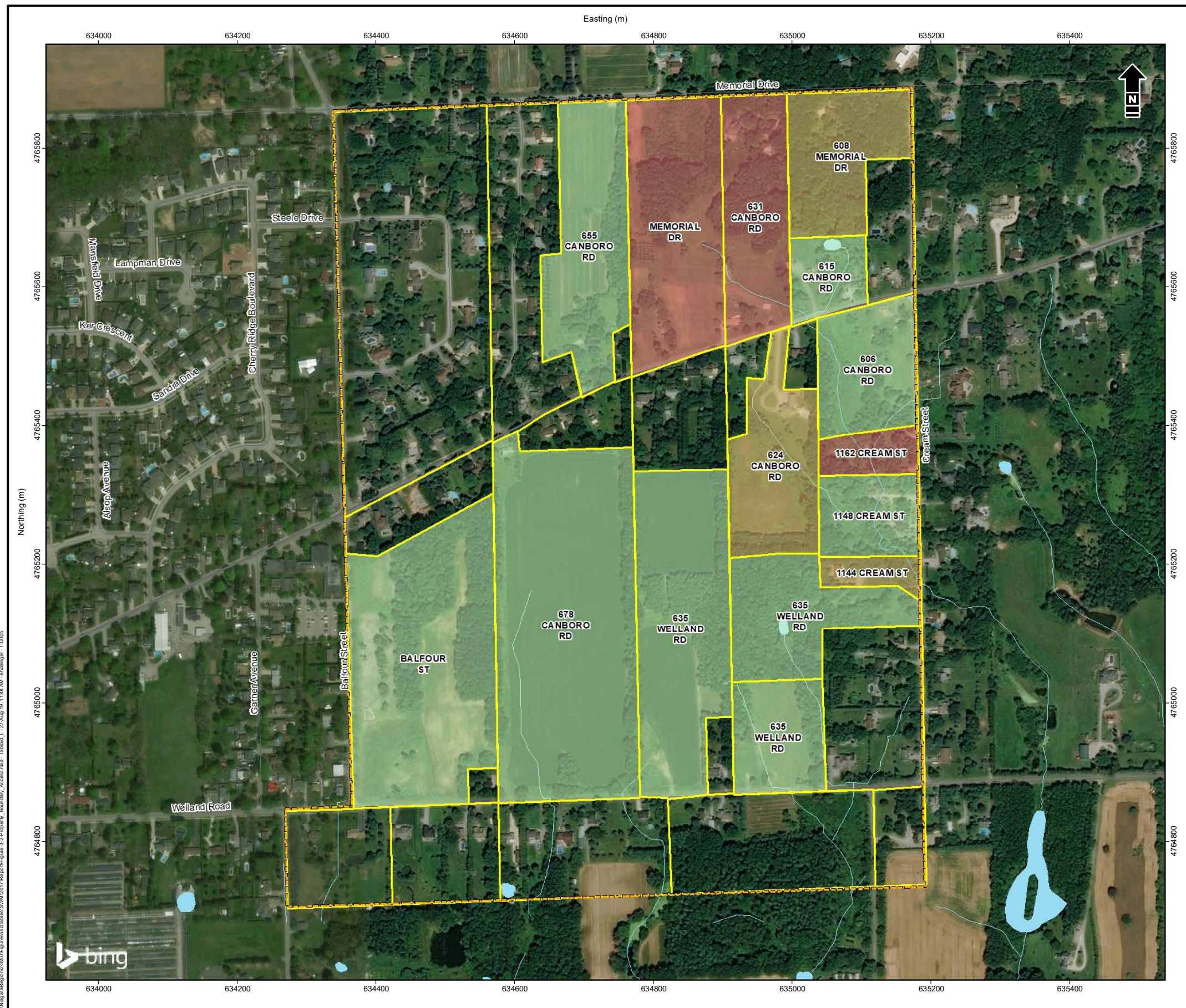
Niagara Region
East Fenwick Secondary Plan

Niagara Peninsula Conservation Authority Planning Review and Regulatory Areas

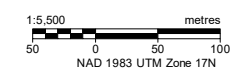
Date: August 2019 Project: 24850 Submitter: E. Wilkinson Reviewer: S. Toner

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- Study Area
- Property Boundary
- Water Body
- Watercourse
- Highway
- Road
- Permission**
- Yes
- Not received
- No



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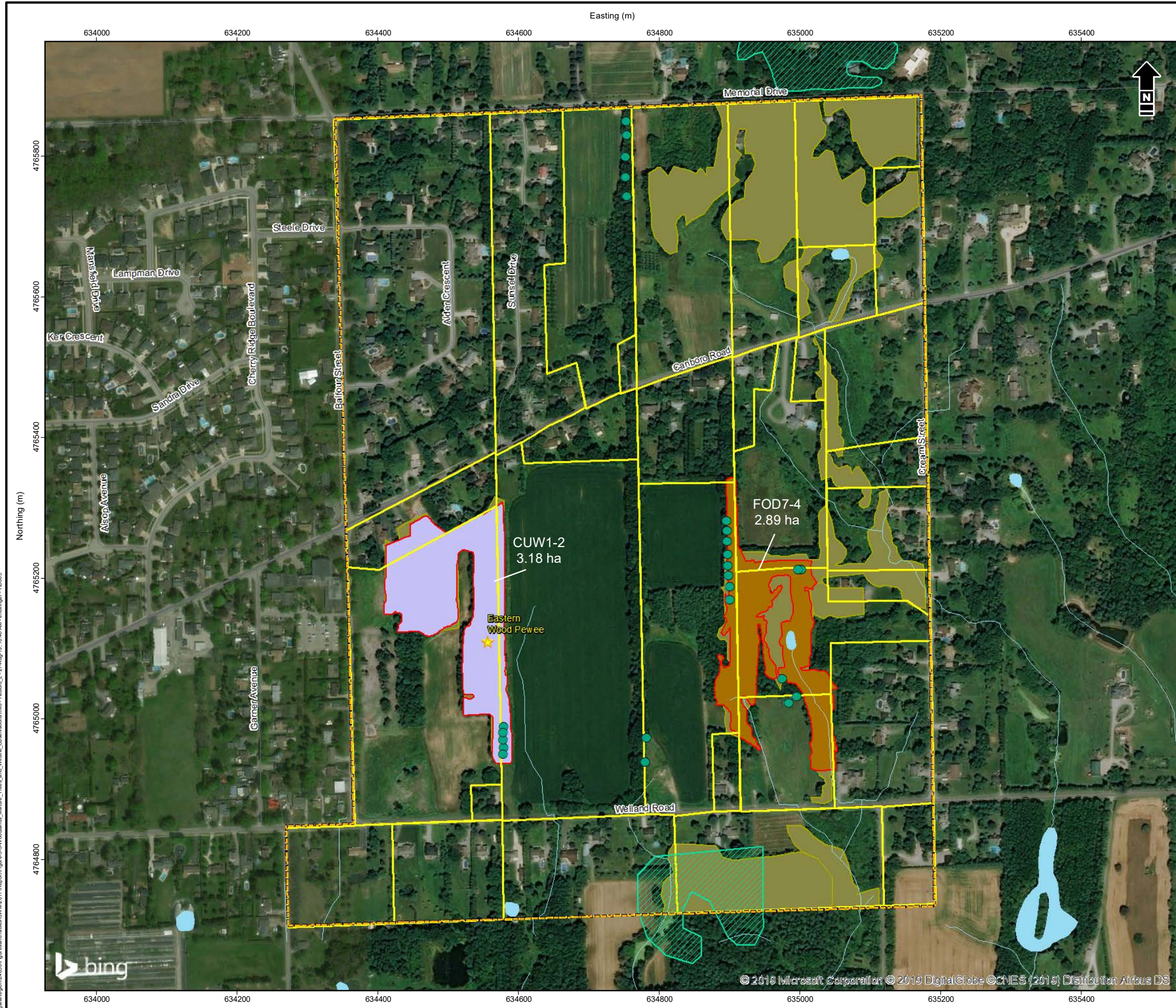


Niagara Region
East Fenwick Secondary Plan

Property DRAFT Access

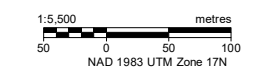
Date: August 2019 Project: 24850 Submitter: E. Wilkinson Reviewer: S. Toner

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- Study Area
- Mature Woodland Unit- Significant
- Mature Cultural Woodland Unit- Significant
- Deer Overwintering Area
- Wooded Area
- Property Boundary
- Water Body
- Watercourse
- Highway
- Road
- Mature Tree Location
- Wildlife Observation

Notes:
 CUW1-2 deemed significant due to it's area being greater than 2 ha (as per Niagara Region OP).
 FOD7-4 deemed significant due to it's area being greater than 2 ha (as per Niagara Region OP) and because it is a rare vegetation community within the upper-tire municipality of Niagara and is therefore considered Significant Wildlife Habitat (protected under the Provincial Policy Statement).



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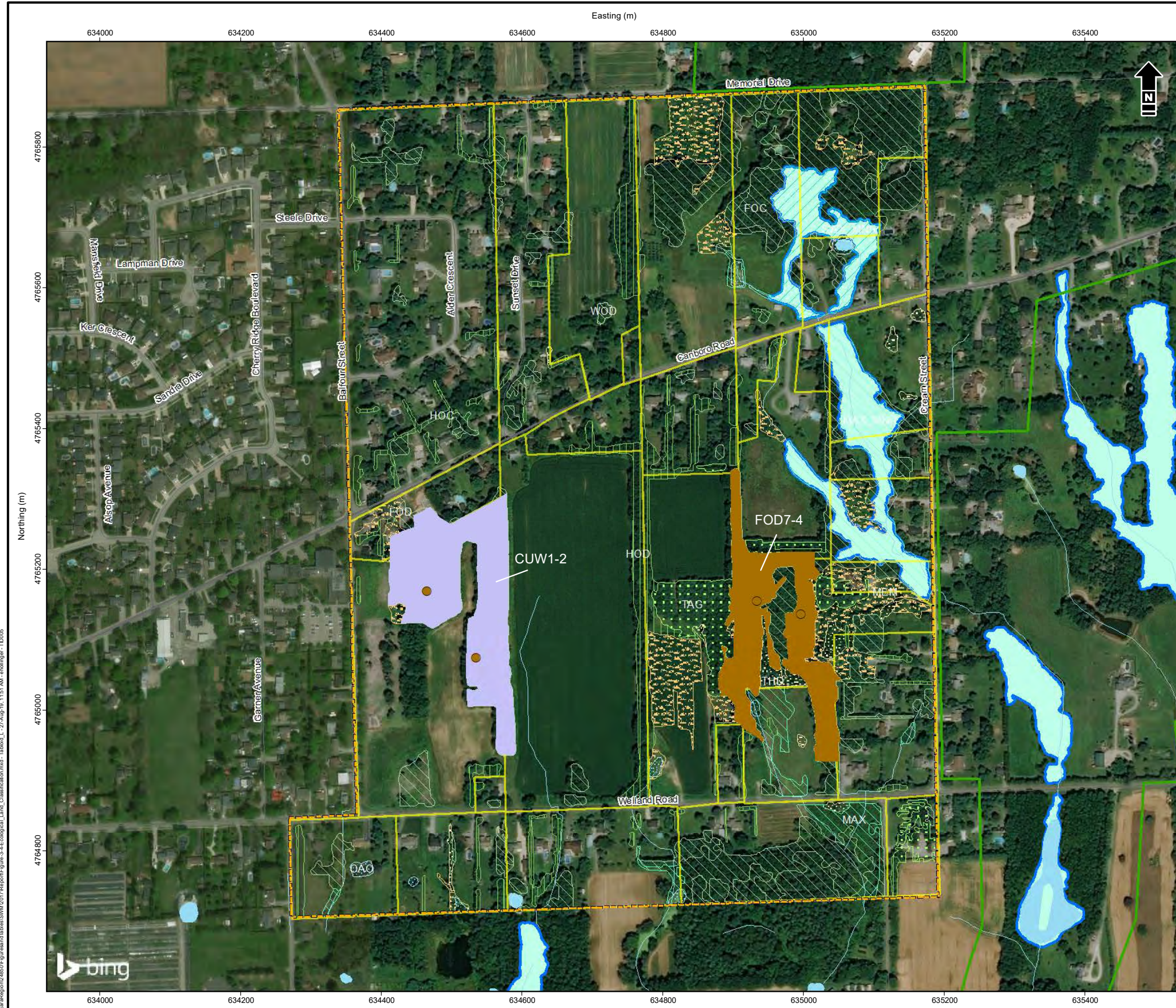


Niagara Region
 East Fenwick Secondary Plan

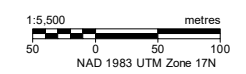
**Woodlands, Mature Trees and
 Wildlife Observations**

Date: August 2019 | Project: 24850 | Submitter: E. Wilkinson | Reviewer: S. Toner

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- Study Area
- Natural Heritage System Area (Undifferentiated)
- Property Boundary
- Provincially Significant Wetland
- Water Body
- Watercourse
- Highway
- Road
- Ecological Land Classification Assessment Location
- Ecological Land Classification Community Class (NPCA)**
- Swamp
- Marsh
- Meadow
- Thicket
- Woodland
- Forest
- Hedgerow
- Treed Agriculture
- Open Water
- Ecological Land Classification Vegetation Community (Matrix)**
- FOD7-4 Fresh-Moist Black Walnut Lowland Deciduous Forest
- CUW1-2 Dry Red Oak Cultural Woodland



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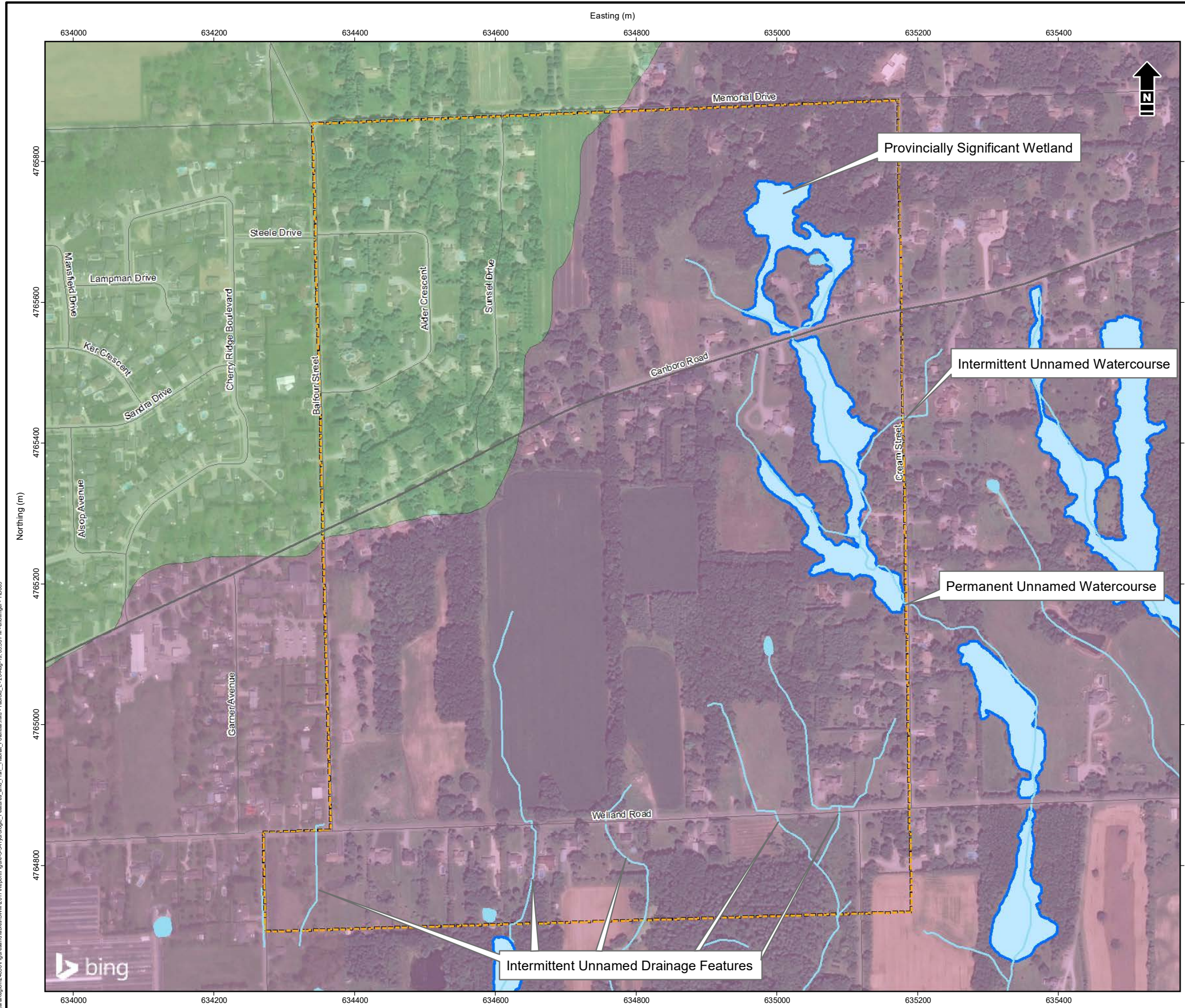


Niagara Region
East Fenwick Secondary Plan

Ecological Land Classification

Date: August 2019 Project: 24850 Submitter: E. Wilkinson Reviewer: S. Toner

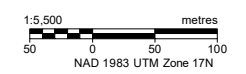
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- Study Area
- Provincialy Significant Wetland
- Fifteen Mile Creek Subwatershed
- Coyle Creek Subwatershed
- Water Body
- Watercourse
- Highway
- Road

Note:
Intermittent Unnamed Drainage features only contribute to downstream fish habitat seasonally (spring freshet) or during high water events.

Permanent Unnamed Watercourse feature contributes to downstream fish habitat regularly within the Coyle Creek Subwatershed.



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Niagara Region
East Fenwick Secondary Plan

Hydrologic Features and Fish Habitat Potential

Date: August, 2019 Project: 24850 Submitter: D. Relyea Reviewer: S. Toner

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APPENDIX E - CULTURAL HERITAGE RESOURCE ASSESSMENT





ENVISIONING FENWICK

CULTURAL HERITAGE RESOURCE ASSESSMENT

EXISTING CONDITIONS REPORT

**EAST FENWICK SECONDARY PLAN CULTURAL HERITAGE STUDY
LOTS 12 & 13, CONCESSION 9 & 10, VILLAGE OF FENWICK**

**TOWN OF PELHAM
NIAGARA REGION, ONTARIO**

Prepared for:

Town of Pelham
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P.O. Box 400
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T: 905-892-2607

ASI File 17CH-038

July 2017



CULTURAL HERITAGE RESOURCE ASSESSMENT

EXISTING CONDITIONS REPORT

EAST FENWICK SECONDARY PLAN CULTURAL HERITAGE STUDY LOTS 12 & 13, CONCESSION 9 & 10, VILLAGE OF FENWICK

TOWN OF PELHAM NIAGARA REGION, ONTARIO

EXECUTIVE SUMMARY

ASI was retained by SGL Planning & Design Inc. on behalf of the Town of Pelham to conduct a Cultural Heritage Resource Assessment (CHRA) Study for the East Fenwick study area in the Town of Pelham, Ontario. The project involves a built heritage and cultural heritage landscape assessment of the subject lands in order to assist the Town of Pelham in the preparation of the East Fenwick Secondary Plan. The aim of this assessment is to guide future development within the 235 acre area of the East Fenwick community.

The purpose of this report is to describe the existing conditions of the study area, present known and identified built heritage and cultural landscapes, and to identify and propose appropriate mitigation measures and recommendations for minimizing and avoiding negative impacts to identified cultural heritage resources, for consideration as a part of the Secondary Plan process.

The results of background historical research and a review of secondary source material, including historical mapping, revealed that the study area has a rural land use history dating back to the early nineteenth century. The field review confirmed that this area retains a number of nineteenth- and twentieth-century cultural heritage resources. 23 cultural heritage resources within and adjacent to the study area have been identified, including: two cultural heritage landscapes, three residential/farmscape properties and 18 residential properties. Land use changes, road improvements and/or pedestrian realm improvements, and other alterations associated with the Secondary Plan process may have a variety of impacts upon cultural heritage resources.

Based on the results of this assessment and a review of the potential heritage impacts resulting from the secondary plan process, the following recommendations have been developed:

1. A total of 23 cultural heritage resources were identified within and/or adjacent to the East Fenwick Secondary Plan study area, which include two cultural heritage landscapes, three residential/farmscape properties and 18 residential properties. The East Fenwick Secondary Plan should incorporate policies that ensure the long-term viability and presence of significant built heritage resources and cultural heritage landscapes.
2. BHR 4 should be considered for designation under Part IV of the Ontario Heritage Act.



3. CHL 2 is important historic rural route and former Indigenous trail, which has been previously identified in the Pelham Heritage Master Plan, and in Section B2.2.7 of the Official Plan. CHL 2 maintains a strong connection to the historic settlement patterns of East Fenwick and to the built heritage resources found along the road. CHL 2 should be studied for designation or recognition as a Cultural Heritage Landscape, under Part V of the Ontario Heritage Act, or through an Official Plan Amendment.
4. CHL 1 is an important historic rural route, characterized by little or no shoulder, no formal ditches, no sidewalks, no curbs, relatively slow, low traffic levels, and mature roadside vegetation. These attributes facilitate pedestrian and cycling activity. CHL 1 acts as a boundary road between the urban and rural areas of East Fenwick. CHL1 should be studied for recognition as a Heritage Route under D4.2.2.6 of the Official Plan.
5. Alternative road and pedestrian realm improvement approaches to conserve and enhance the road corridors and alternative development and land use approaches that conserve and enhance the landscape and built form character of adjacent properties should be considered for CHL 1 and CHL 2 as a part of the secondary plan process. A gradual transition from the urban boundary to the rural areas to the north should be considered for CHL 1.
6. BHRs 2, 8, 16 and 20 contain heritage features that are good candidates for conservation. Based on a review of the East Fenwick Secondary Plan Area Conceptual Tertiary Plan, prepared by Upper Canada Consultants in 2010, and consideration of properties then shown to be owned by a developer's group, it is expected that these properties may be subject to impacts as a result of future development or land use changes. A Heritage Impact Assessment should be completed for the subject properties.
7. CHLs 1-2 and BHRs 1- 2, 4-21 may be altered as a result of changes in land use, future development, road widening and/or improvements and pedestrian realm improvements. Upon the completion of a proposed land use plan resulting from the secondary plan process, the heritage impacts should be assessed, including the development of specific mitigation measures.



ARCHAEOLOGICAL SERVICES INC.

PROJECT PERSONNEL

<i>Senior Project Manager:</i>	Rebecca Sciarra, MA, CAHP Partner and Director, Business Services
<i>Project Manager:</i>	Lauren Archer, BA Cultural Heritage Specialist, Cultural Heritage Division
<i>Project Administrator:</i>	Carol Bella, Hons BA Research Archaeologist and Administrative Assistant
<i>Fieldwork Assistant:</i>	Laura Wickett, BA (Hon), Dip. Heritage Conservation Cultural Heritage Assistant, Cultural Heritage Division
<i>Report Preparation:</i>	Lauren Archer Laura Wickett
<i>Graphics:</i>	Jonas Fernandez, MA Geomatics Specialist
<i>Report Reviewer:</i>	Rebecca Sciarra



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1.0 INTRODUCTION

ASI was retained by SGL Planning & Design Inc. on behalf of the Town of Pelham to conduct a Cultural Heritage Resource Assessment (CHRA) Study for the East Fenwick study area in the Town of Pelham, Ontario (Figure 1). The project involves a built heritage and cultural heritage landscape assessment of the subject lands in order to assist the Town of Pelham in the preparation of the East Fenwick Secondary Plan. The aim of this assessment is to guide future development within the 235 acre area of the East Fenwick community.

The purpose of the CHRA report is to describe the existing conditions of the study area, present a built heritage and cultural landscape inventory of cultural heritage resources, and propose appropriate mitigation measures and recommendations for minimizing and avoiding negative impacts on identified cultural heritage resources. The assessment was completed by Lauren Archer, Cultural Heritage Specialist in the Cultural Heritage Division at ASI.

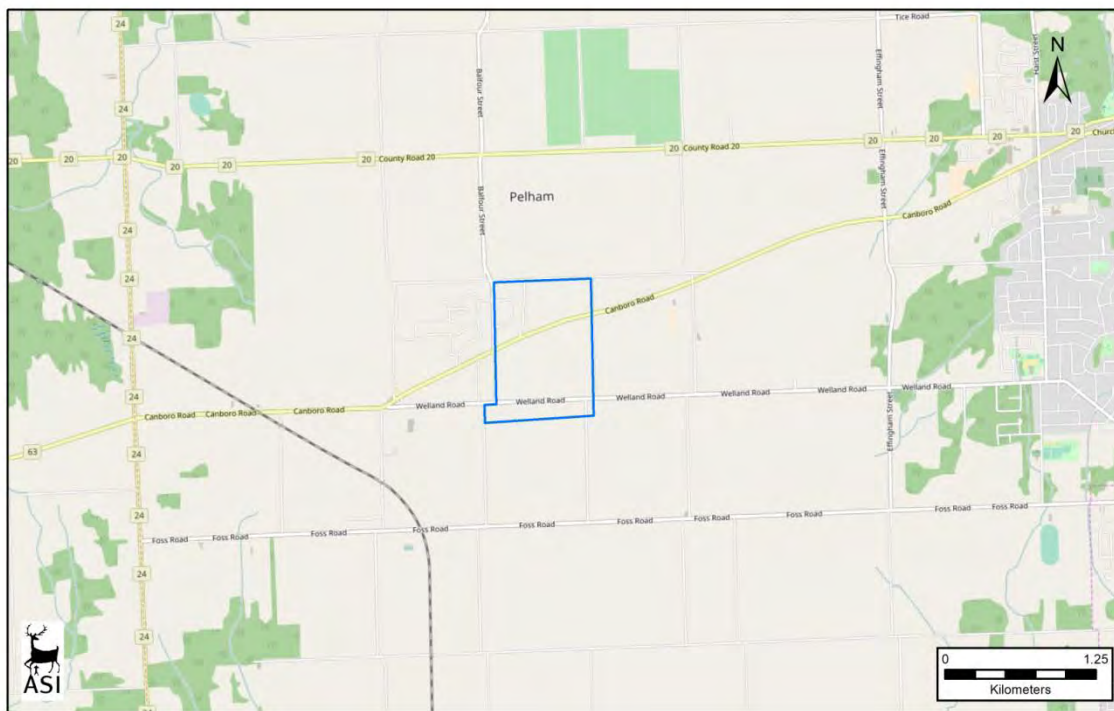


Figure 1: Location of the East Fenwick study area

2.0 BUILT HERITAGE RESOURCE AND CULTURAL HERITAGE LANDSCAPE ASSESSMENT CONTEXT

2.1 Legislation and Policy Context

The authority to request this heritage assessment arises from Section 2 (d) of the *Planning Act*. The *Planning Act* (1990) and related *Provincial Policy Statement (PPS)*, which was updated in 2014, make a number of provisions relating to heritage conservation. One of the general purposes of the *Planning Act* is to integrate matters of provincial interest in provincial and municipal planning decisions. In order to

inform all those involved in planning activities of the scope of these matters of provincial interest, Section 2 of the *Planning Act* provides an extensive listing. These matters of provincial interest shall be regarded when certain authorities, including the council of a municipality, carry out their responsibilities under the *Act*. One of these provincial interests is directly concerned with:

- 2.(d) the conservation of features of significant architectural, cultural, historical, archaeological or scientific interest

Part 4.7 of the *PPS* states that:

The official plan is the most important vehicle for implementation of this Provincial Policy Statement. Comprehensive, integrated and long-term planning is best achieved through official plans.

Official plans shall identify provincial interests and set out appropriate land use designations and policies. To determine the significance of some natural heritage features and other resources, evaluation may be required.

Official plans should also coordinate cross-boundary matters to complement the actions of other planning authorities and promote mutually beneficial solutions. Official plans shall provide clear, reasonable and attainable policies to protect provincial interests and direct development to suitable areas.

In order to protect provincial interests, planning authorities shall keep their official plans up-to-date with this Provincial Policy Statement. The policies of this Provincial Policy Statement continue to apply after adoption and approval of an official plan.

Those policies of particular relevance for the conservation of heritage features are contained in Section 2-Wise Use and Management of Resources, wherein Subsection 2.6 - Cultural Heritage and Archaeological Resources, makes the following provisions:

- 2.6.1 Significant built heritage resources and significant cultural heritage landscapes shall be conserved.

A number of definitions that have specific meanings for use in a policy context accompany the policy statement. These definitions include built heritage resources and cultural heritage landscapes.

A *built heritage resource* is defined as: “a building, structure, monument, installation or any manufactured remnant that contributes to a property’s cultural heritage value or interest as identified by a community, including an Aboriginal community” (PPS 2014).

A *cultural heritage landscape* is defined as “a defined geographical area that may have been modified by human activity and is identified as having cultural heritage value or interest by a community, including an Aboriginal community. The area may involve features such as structures, spaces, archaeological sites or natural elements that are valued together for their interrelationship, meaning or association” (PPS 2014). Examples may include, but are not limited to farmscapes, historic settlements, parks, gardens, battlefields, mainstreets and neighbourhoods, cemeteries, trailways, and industrial complexes of cultural heritage value.



In addition, significance is also more generally defined. It is assigned a specific meaning according to the subject matter or policy context, such as wetlands or ecologically important areas. With regard to cultural heritage and archaeology resources, resources of significance are those that are valued for the important contribution they make to our understanding of the history of a place, an event, or a people (*PPS 2014*).

Criteria for determining significance for the resources are recommended by the Province, but municipal approaches that achieve or exceed the same objective may also be used. While some significant resources may already be identified and inventoried by official sources, the significance of others can only be determined after evaluation (*PPS 2014*).

Accordingly, the foregoing guidelines and relevant policy statement were used to guide the scope and methodology of the cultural heritage assessment.

2.2 Town of Pelham Policies Regarding Cultural Heritage

The Town of Pelham provides cultural heritage policies in Section D4 of its Official Plan (2012). Cultural heritage policies relevant to this assessment are provided below:

D4.2.1 Cultural Heritage Impact Statements

Council may require the submission of a Heritage Impact Assessment (HIA) to support an application for development if the affected lands are the site of an identified cultural heritage resource or are located in close proximity to an identified cultural heritage resource. The intent of the HIA is to determine what impacts the development will have on the resource and whether the application for development will conform to the goals, objectives and policies of this Plan.

D4.2.2.1 Built Heritage Register

Under Section 27 of the Ontario Heritage Act, the Town must maintain a register of all designated properties, but Council may also include on the register, properties that have not been designated but that Council believes to be of cultural heritage value or interest.

D4.2.2.2 Cultural Heritage Landscape Register

In accordance with the Ontario Heritage Act, the Town must also prepare an inventory of cultural heritage landscapes. Landscapes such as existing rural and agricultural areas, historic hamlets, and heritage roads will be identified in the inventory. A cultural heritage landscape is a defined geographical area of heritage significance that has been modified by human activities. Such an area is valued by a community and is of significance to the understanding of the history of a people or place.

The Town of Pelham also has a Heritage Master Plan, which was completed in August 2012. The Heritage Master Plan guides the Town's plans for finding, assessing, conserving and celebrating heritage resources. It encourages development that respects the heritage character of Pelham, recommends policies for inclusion in the Town's Official Plan and provides priorities and timelines for the Town's actions in heritage conservation.



D4.2.2.6 Heritage Routes

Council supports the development of Heritage Routes that weave through the Town's Rural Area, providing linkages for hiking, cycling, and car touring and highlighting cultural heritage resources. Selected Heritage Routes are to be identified in consultation with the Pelham Municipal Heritage Committee.

In support of developing the Heritage Routes, Council shall endeavour to:

- a) Prepare streetscape guidelines or standards to protect cultural heritage features and resources along heritage routes. The design guidelines will provide protection for existing trees and landscape features, and will ensure that the general heritage appeal and views are protected and enhanced;
- b) Coordinate clear and consistent signage along the Heritage Routes that may serve wayfinding and/or educational purposes;
- c) Cooperate with the Region, adjacent municipalities, and the Wine Council of Ontario to ensure Heritage Route signage is coordinated with any other local signage (e.g. for Wine Routes);
- d) Support the development of appropriate scenic lookouts and other complementary uses along Heritage Routes, provided that such uses:
 - i) Are small in scale;
 - ii) Are in keeping with, and complementary to the passive recreational character of the Route;
 - iii) Have no negative impacts on the surrounding public and/or private land uses;
 - iv) Have no negative impacts on the natural environment or on cultural heritage resources; and v) Will not require the extension of the municipal water supply or sanitary sewage services.
- e) Enhance cycling and driving conditions along the Heritage Route corridors where appropriate, including through the provision of bicycle lanes in accordance with the Niagara Region Bicycling Network. Wherever possible, linkages to other recreational driving routes and cycling/hiking trails in the Region should be achieved.

To support the tourism role of Heritage Routes, the Town will promote the Heritage Routes and request that the Niagara Economic and Tourism Corporation include the Pelham Heritage Routes in its promotional materials and activities.

Section B2 of the Official Plan addresses Rural Area Designations, section B2.2 Specialty Agricultural includes the identification of the Canboro Road Corridor as a special area in the Town of Pelham:

B2.2.7 The Canboro Road Corridor

The Canboro Road corridor is an important transportation linkage between Downtown Fenwick and Fonthill and is considered to be an area of significant potential for enhancement as a rural promenade characterized by public parks and spaces geared to pedestrians and cyclists, as well as the promotion of agricultural based tourism and accessory commercial uses. In an effort to encourage and foster land use that contributes to the identity of a promenade, the Zoning By-law may establish site-specific provisions for agricultural-related and secondary uses along this corridor. In addition, the Canboro Road corridor between Fenwick and Fonthill shall be defined



as a Community Improvement Plan study area should Council wish to use the provisions of a Community Improvement Plan to foster and enhance this area as a promenade and tourist destination.

2.3 East Fenwick Secondary Plan Context

The Town of Pelham Council has identified the development of a Secondary Plan for East Fenwick as a priority. The Secondary Plan will guide future growth and development in East Fenwick, including the greenfield area. The Secondary Plan will be adopted by Town Council and approved by the Region of Niagara.

The project goals are to develop a Secondary Plan for the East Fenwick urban area that will provide the framework:

- for permitting new development compatible with the character of Fenwick and consistent with Provincial, Regional and Local planning policies and legislation;
- to establish appropriate land use designations and policies that will support the future development of East Fenwick for an appropriate and compatible mix of uses, local services and community infrastructure;
- to identify existing transportation and infrastructure, i.e. water, sanitary and storm services, available in and around the study area and upgrades that may be required to the systems to support the Secondary Plan;
- to establish a system of public space areas and linkages with natural heritage areas;
- to provide for an implementation and phasing plan;
- to establish design guidelines for built form and public realm development that is consistent with the policy objectives of the Secondary Plan and the provision of well-designed attractive; and accessible public spaces that prioritize the pedestrian experience.

The study area includes that area within the urban area boundary of Fenwick that is bounded by Memorial Drive to the north, Balfour Street to the west, land on the south side of Welland Road to a depth of approximately 120m to the south and Cream Street to the east and comprises approximately 95 ha (235 acres). Canboro Road bisects the study area and is identified as an arterial road and Welland Road along the south boundary is considered to be a collector road with all the other streets being local roads.

The lands are designated Urban Living Area/Built Boundary with the majority of the area identified within the Greenfield Overlay and a portion of the area is designated Environmental Protection Three in the Town Official Plan. The Official Plan also identifies provincially significant wetlands, woodlands and deer wintering area within the study area. Also, the study area is included within an area designated as being part of a highly vulnerable aquifer. The Region of Niagara Official Plan also identifies a significant portion of the Secondary Plan area as a designated greenfield area with the remainder as built up along with environmental protection and environmental conservation areas relating to provincially significant wetlands and significant woodlands.

2.4 Data Collection

In the course of the cultural heritage assessment, all potentially affected cultural heritage resources within the study area are subject to inventory. Short form names are usually applied to each resource type, (e.g. barn, residence). Generally, when conducting a preliminary identification of cultural heritage resources,



three stages of research and data collection are undertaken to appropriately establish the potential for and existence of cultural heritage resources in a particular geographic area.

Background historic research, which includes consultation of primary and secondary source research and historic mapping, is undertaken to identify early settlement patterns and broad agents or themes of change in a study area. This stage in the data collection process enables the researcher to determine the presence of sensitive heritage areas that correspond to nineteenth and twentieth century settlement and development patterns. To augment data collected during this stage of the research process, federal, provincial, and municipal databases and/or agencies are consulted to obtain information about specific properties that have been previously identified and/or designated as retaining cultural heritage value. Typically, resources identified during these stages of the research process are reflective of particular architectural styles, associated with an important person, place, or event, and contribute to the contextual facets of a particular place, neighbourhood, or intersection.

A field review is then undertaken to confirm the location and condition of previously identified cultural heritage resources. The field review is also utilized to identify cultural heritage resources that have not been previously identified on federal, provincial, or municipal databases.

Several investigative criteria are utilized during the field review to appropriately identify new cultural heritage resources. These investigative criteria are derived from provincial guidelines, definitions, and past experience. A built structure or landscape is identified as a cultural heritage resource that should be considered during the course of the assessment, if the resource meets a combination of the following criteria:

- It is 40 years or older;
- It is a rare, unique, representative or early example of a style, type, expression, material or construction method;
- It displays a high degree of craftsmanship or artistic merit;
- It demonstrates a high degree of technical or scientific achievement;
- The site and/or structure retains original stylistic features and has not been irreversibly altered so as to destroy its integrity;
- It has a direct association with a theme, event, belief, person, activity, organization, or institution that is significant to: the Town of Pelham; the Province of Ontario; Canada; or the world heritage list;
- It yields, or had the potential to yield, information that contributes to an understanding of: the Town of Pelham; the Province of Ontario; Canada; or the world heritage list;
- It demonstrates or reflects the work or ideas of an architect, artist builder, designer, or theorist who is significant to: the Town of Pelham; the Province of Ontario; Canada; or the world heritage list;
- It is important in defining, maintaining, or supporting the character of an area;
- It is physically, functionally, visually, or historically linked to its surroundings;
- It is a landmark;
- It illustrates a significant phase in the development of the community or a major change or turning point in the community's history;
- The landscape contains a structure other than a building (fencing, culvert, public art, statue, etc.) that is associated with the history or daily life of that area or region; or
- There is evidence of previous historic and/or existing agricultural practices (e.g. terracing, deforestation, complex water canalization, apple orchards, vineyards, etc.).



If a resource satisfies an appropriate combination of these criteria, it will be identified as a cultural heritage resource and is subject to further research where appropriate and when feasible. Typically, further historical research and consultation is required to determine the specific significance of the identified cultural heritage resource. When identifying cultural heritage landscapes, the following categories are typically utilized for the purposes of the classification during the field review:

Farmscapes:	comprise two or more buildings, one of which must be a farmhouse or barn, and may include a tree-lined drive, tree windbreaks, fences, domestic gardens and small orchards.
Roadscapes:	generally two-lanes in width with absence of shoulders or narrow shoulders only, ditches, tree lines, bridges, culverts and other associated features.
Waterscapes:	waterway features that contribute to the overall character of the cultural heritage landscape, usually in relation to their influence on historic development and settlement patterns.
Railscapes:	active or inactive railway lines or railway rights of way and associated features.
Historical Settlements:	groupings of two or more structures with a commonly applied name.
Streetscapes:	generally consists of a paved road found in a more urban setting, and may include a series of houses that would have been built in the same time period.
Historical Agricultural Landscapes:	generally comprises a historically rooted settlement and farming pattern that reflects a recognizable arrangement of fields within a lot and may have associated agricultural outbuildings and structures
Cemeteries:	land used for the burial of human remains.

3.0 HISTORICAL CONTEXT

3.1 Introduction

This section provides a brief summary of historical research and a description of both previously identified above ground cultural heritage resources as well as cultural heritage resources that have been identified through fieldwork, which may be affected by the proposed undertaking. A review of available primary and secondary source material was undertaken to produce a contextual overview of the study area, including a general description of Euro-Canadian settlement and land use. Historically, the study area is located in the Former Township of Pelham, Welland County in the following lots:

- Lots 12 and 13, Concession 9-10



3.2 Natural Heritage, Geography and Physiography

East Fenwick is situated within the Haldimand Clay Plain physiographic region of southern Ontario (Chapman and Putnam 1984: 156–159). The Haldimand Clay Plain physiographic region, an area of approximately 3,500 square kilometres, comprises the majority of the Niagara Peninsula south of the Niagara Escarpment, the limestone bluffs of which channeled early settlement, especially in the Short Hills, along Twelve Mile Creek.

In the last Ice Age, the region was entirely submerged by glacial Lake Warren which now consists of predominantly glaciolacustrine clay overburden. The depth and even the presence of this clay overburden varies from place to place, and there are many relatively distinct sub-areas of the region. Predominant native vegetation at time of settlement included a mixed hardwood/coniferous climax forest of American Chestnut, White Pine, White and Red Oak, Beech, Sugar Maple, Black and White Ash (Cruikshank, 1887: 290). The study area is located between the watersheds of the Welland River to the south, and Fifteen Mile Creek to the north. The Fonthill Kame is recognized as a proglacial delta of the ancient Lake Warren. The Fonthill Kame influences the climate of Pelham by sheltering it from the winds from the southwest. This provides good growing conditions for fruit crops, including the vines that supply the local wine industry. It is also mined for sand and gravel. The best growing soil is in the area flanking Canboro Road between Fenwick and Fonthill, in the Fonthill Kame, which is optimal for fruit production.

The area is representative of a number of Carolinian species and is home to over 500 bog, valley, and meadow plant species. The habitat within the Kame is also well suited for a variety of animal species including the white-tailed deer, opossum, red fox, and meadow voles. It also serves as a feeding and sanctuary area for wood ducks, green-winged teals, mallard and black ducks, and great blue herons. The Fonthill Kame is also home to some rare and threatened species including the spotted salamander, red-backed salamander, pickerel frog, pileated woodpecker, and the spotted turtle. Also prevalent are rare plant species including ginseng, brook-beech fern, flowering dogwood, black walnut, swamp white oak, sassafras, tulip tree and the cucumber tree. (Niagara Greenbelt, 2016)

3.3 Indigenous Land Use

The lands with the East Fenwick study area have a cultural history which begins approximately 11,000 years ago and continues to the present. Although East Fenwick does not appear to have been occupied by aboriginal groups on any permanent basis, it has primarily been used by early Indigenous people as an important travel route, through which parties of Indigenous peoples passed during certain times of the year. The land did not offer sufficient resources to support year-round occupation. Based on an inventory of documented archaeological sites, it appears that the extensive clay plains of the mid-peninsular area may have prevented Iroquoian peoples from establishing villages in this area (BRAY Heritage 2011). The majority of archaeological material from the pre-contact period represents the remains of small camps occupied for short periods of time as people moved throughout their territories on a seasonal basis. Oral narratives identify Canboro Road as an old Indigenous trail, along with Lundy's Lane (to the east) and the Talbot Road (to the west), which both connect with the Canboro Road (Brehault 1968: 14). A few small component sites may represent the traces of parties travelling between the major clusters of large Neutral settlements in the Hamilton-Brantford-Grimsby area to the west and the Fort Erie-Port Colborne area to the east (BRAY Heritage 2011).



3.4 Township Survey and Settlement

3.4.1 County of Welland

From 1841 to 1851 the entire Niagara Peninsula comprised a single county municipality, administered by a District Council whose headquarters were located at Niagara-on-the-Lake. During that time Welland County was part of Lincoln County, one of the nineteen counties created by Upper Canada's first Lieutenant-Governor, John Graves Simcoe, in 1792. (Mika and Mika 1977)

In 1851 the southern section of the district broke away to be governed by a provisional council under Warden John Fraser. The town of Welland, then known as Merrittsville, was chosen as the county seat in 1854, and here the first Welland County Council met at the new county buildings on August 18, 1856. (Mika and Mika 1977)

Welland County's early settlers were United Empire Loyalists who came to the area during and after the American Revolutionary War. Many of them were disbanded soldiers of "Butler's" Rangers, a corps of Loyalist refugees raised by Lt. Col. John Butler and led by him in numerous forays from the Niagara Peninsula into rebel territory. The building of the first Welland Canal in the 1820's stimulated the growth of settlements in the area. (Mika and Mika 1977)

3.4.2 Township of Pelham

Located centrally within the Niagara Peninsula, the Town of Pelham was, until January 1, 1970, the Township of Pelham. At that time its main centres of population were Fonthill, Ridgeville, Fenwick, and North Pelham. The Welland River forms the southern boundary of the town and Highway 20 cuts laterally across the area. The Toronto, Hamilton and Buffalo Railway runs across the southern portion of the town. (Mika and Mika 1977)

Settlement of the region began about 1784, the earliest inhabitants including David Secord, George Hansler, Jacob Reece, John Wenger, Samuel Beckett-Willson, John Crowe, A. Overholt, T. Page and T. Rice, as well as settlers Disher, Wilson, Miller, Nunn, Jennings, Foss and Oille. Many of these were Quakers from Pennsylvania and other parts of the United States. (Mika and Mika 1977)

Fonthill was originally known as Riceville, then Osborne's Corners, and Temperanceville, before receiving the name Fonthill in 1850. A post office was established in 1841 under the name of Pelham. John Price was first postmaster. A Baptist church was built in 1846, and the Fonthill Grammar School in 1856. Industries at Fonthill included the Fonthill Nurseries, established in 1837. The area continues to be an important area for fruit crops today. (Mika and Mika 1977)

By 1886 The Township of Pelham contained two woollen mills, three post offices, seven churches, ten public schools, and at least one nursery. Railway travel came to Pelham in 1853 when a Great Western Railway train made its first journey from Hamilton to Queenston.

On January 1, 1970 Pelham Township became the Town of Pelham. The region's rich soil and temperate climate support the growth of fruit and other cash crops. (Mika and Mika 1977)



3.4.3 Village of Fenwick

Situated on the historic Canboro Road, Fenwick was originally a police village in Pelham Township, Welland County. In 1970 Lincoln and Welland Counties were amalgamated to form the Regional Municipality of Niagara, and Fenwick was joined to the Village of Fonthill and the Township of Pelham and became a part of the newly created Town of Pelham. The town is situated in a rich market gardening and fruit-producing area. Prior to annexation, the population of Fenwick was 737. (Mika and Mika 1977)

Fenwick was first settled in the early 1790s, when it was known as Diffin's Corners. In 1853 a post office opened in the settlement and the name officially changed to Fenwick, likely in honour of a British nobleman. Sometime in the 1860s the settlement was incorporated as a police village. (Mika and Mika 1977)

Many of the original settlers in the area were United Empire Loyalists and Quakers who had emigrated from Pennsylvania and New Jersey. The early churches of Fenwick included the Episcopalian Methodist and the New Connexion Methodist. In 1977 there were three churches in Fenwick, the United Church, whose history began in 1835, with the present building erected in 1900, the Church of Christ, and St. Ann's Roman Catholic Church. In 1955 the Polish congregation built the latter church and services were conducted in Polish. (Mika and Mika 1977)

Fenwick had two hotels, the usual small businesses necessary to serve a pioneering community, and later, an Oddfellow's Hall, and the Lion's Club, which, in more recent times officially opened Centennial Park. Memorial School was named for the much respected Ed. Farr, an early school teacher. The Fenwick Fall Fair, under the auspices of the Pelham Township Agricultural Society, was held in Fenwick in the middle 1850s. Excursion trains brought people from Fort Erie, Hamilton and Toronto. Early industries in Fenwick included an apple-drying factory, a spinning mill, a sawmill, a cooperage and several blacksmith shops (Mika and Mika 1977).

3.4.4 Canboro Road Corridor

The Canboro Road, (sometimes “Canborough”, or the “Great West Road”), was the route along which the first settlements in Pelham were established, which initially centered around coach stops/inns, but also around early institutions. Historic mapping suggests that the Canboro Road was not actually surveyed and improved as a road until the 1840s (Cruikshank 1887: 293). Oral narratives identify Canboro Road as an old Indigenous trail, along with Lundy’s Lane (to the east) and the Talbot Road (to the west), which both connect with the Canboro Road (Brehault 1968: 14). The best growing soil is in Pelham flanking Canboro Road between Fenwick and Fonthill, within the Fonthill Kame, and is optimal for fruit production. Canboro Road runs diagonally through the historically surveyed roads that together form the historic road network which together formed the early infrastructure of Fenwick, and connected the area regionally. Additional historic corridors within the study area include Memorial Drive and Welland Road.

The Canboro Road Corridor has been identified as an important historic scenic route and grouping of heritage resources in the Town of Pelham Heritage Master Plan (BRAY Heritage 2012). Character defining elements include its diagonal alignment cutting across the concession grid, the component communities, including Fenwick, the cemeteries and community buildings along the road and the mature roadside vegetation. The Canboro Road Corridor benefits from slow traffic speeds and low-medium volumes of vehicular traffic. The large roadside trees and adjacent buildings provide the elements of a scenic drive and set it apart from roads that serve as traffic arteries (BRAY Heritage 2012).



3.5 Review of Historic Mapping

A number of property owners and historical features are illustrated within the study area within the 1862 *Tremaine Map of the Counties of Lincoln and Welland* and the 1880 *Illustrated Historical Atlas of the Township of Pelham*. It should be noted, however, that not all features of interest were mapped systematically in the Ontario series of historical atlases, given that they were financed by subscription, and subscribers were given preference with regard to the level of detail provided on the maps. Moreover, not every feature of interest would have been within the scope of the atlases. Table 1 provides a summary of early land owners and tenants, as well as identified historical features.

Historic mapping within the 1862 *Tremaine Map of the Counties of Lincoln and Welland* (Figure 2) confirmed that the study area was a rural, agricultural landscape in the mid-nineteenth century. In addition, historic map analysis demonstrates that Memorial Drive, Balfour Street, Welland Road, Cream Street and Canboro Road were surveyed prior to 1859. The maps reviewed record the names of owners/occupants of properties within the study area, as well as the location and arrangement of residences, farmhouses, churches, schools and other key resources. The map depicts the location of the historic settlement area of Fenwick, to the west of the study area, including the Fenwick Post Office.

By 1880, the *Illustrated Historical Atlas of the Township of Pelham* (Figure 3) indicates that several properties have changed hands, and have been severed, however, the area is still a predominantly rural agricultural area. Farmsteads with farmhouses and orchards exist on many properties, and a church exists at the intersection Canboro Road and Belfour Street. The map depicts the course of a tributary of the Welland River. The historic settlement area of Fenwick, to the west, has experienced some growth at the intersection of Canboro Road and Welland Road.

National Topographic Survey (NTS) mapping from 1907, 1920, and 1938 as well as aerial photography from 1954 illustrates the development of the study area over the course of the early twentieth century. Generally, this mapping demonstrates a period of steady but limited growth of the historic settlement area of Fenwick east along Canboro Road. The area largely retains its rural agricultural character to the current day. Alder Crescent and Sunset Crescent are both later developments, occurring after 1954 but before 2007.

In the National Topographic Survey (NTS) mapping from 1907 (Figure 4), light development of farmhouses and residences exists along Canboro Road, Cream Street, and to a lesser extent Welland Road and Memorial Drive. The historic settlement of Fenwick has grown along Canboro Road and north to Maple Street. This level of development stays consistent in the 1920 National Topographic Survey (NTS) mapping. The 1920 mapping indicates that (Figure 5) the area retains its rural agricultural character, very little changes in the study area.

In the National Topographic Survey (NTS) mapping from 1938 (Figure 6), the area retains its rural agricultural character. However, the historic settlement of Fenwick, along Canboro Road, has experienced steady but significant growth, increasing in density until Belfour Road, which was the traditional urban boundary of Fenwick. Additional residential development has also occurred along Cream Street and Welland Road.

In the Digital Aerial Photograph of Southern Ontario from 1954 (Figure 7), the area retains its rural agricultural character. Very little development has occurred beyond the historic agricultural and rural hamlet historic settlement patterns of Fenwick.



Table 1: Nineteenth-century Property Owners and Historical Features in the Study Area

Location		Tremaine	Illustrated Atlas
Con	Lot	Owner(s)/Tenant(s)	Historical Feature(s)
9	12	T.C. J.D. J. Fell J. Wellson J. Crow	Buildings (1) (Along Canboro Road) E.S. W.M. Mrs. B J.S. A.B. B.F.
	13	E.Mch. R. Farr G.W. Wulers J. Hicks	Buildings (1) D.F.S. R.F. R. Farr C. Reece Geo. Cplar T.H.
10	12	U. Rice J. Fliey G. Castle	Buildings (3) T. Scanton
	13	Geo. Waters	n/a D. Leppert

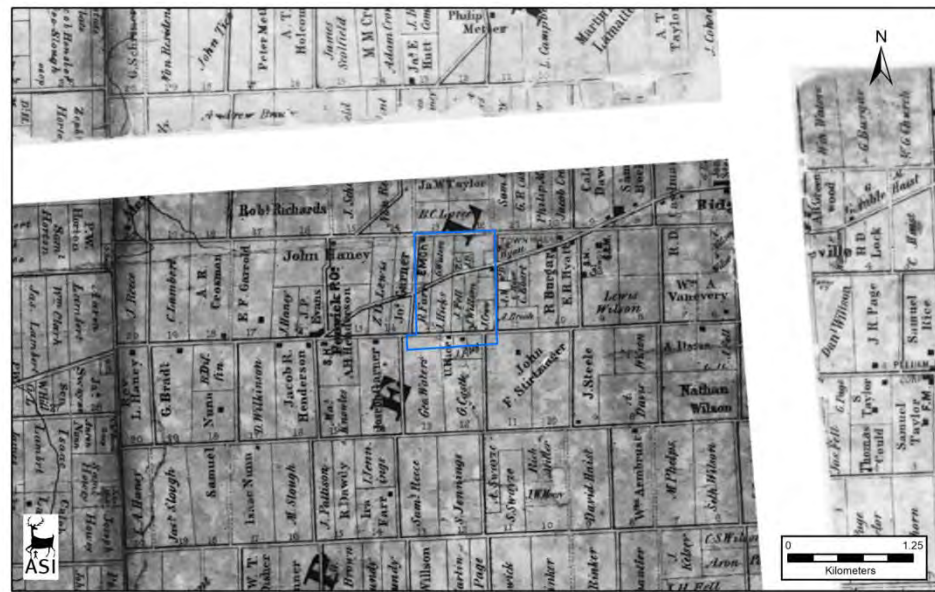


Figure 2: 1862 Tremaine's Map of the Counties of Lincoln and Welland
 (Source: Tremaine 1862)

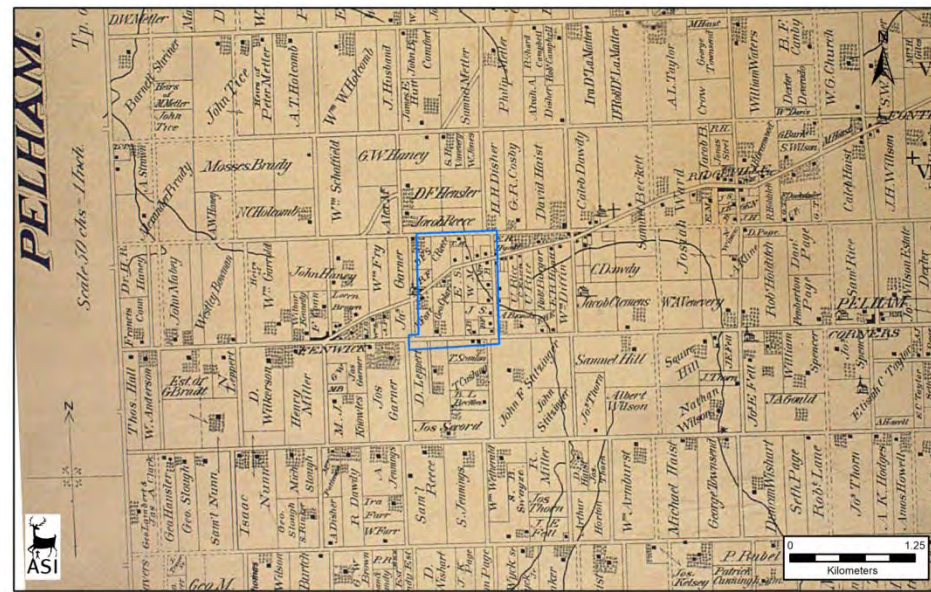


Figure 3: 1880 *Historic Atlas Map of Pelham Township*
 (Source: Pope 1880)

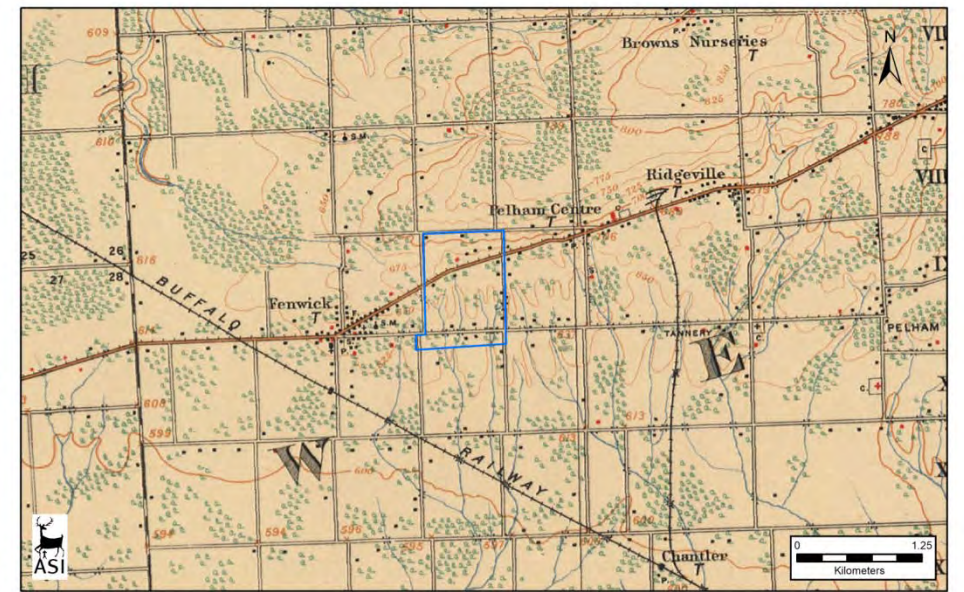


Figure 4: 1907 National Topographic Survey (NTS)
 (Source: NTS 1907)

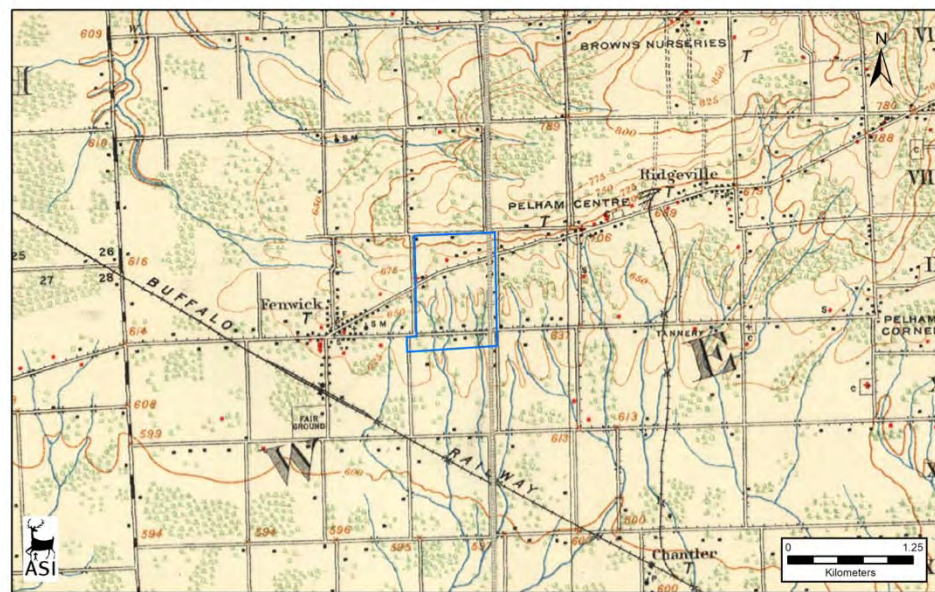


Figure 5: 1920 National Topographic Survey (NTS)
 (Source: NTS 1920)

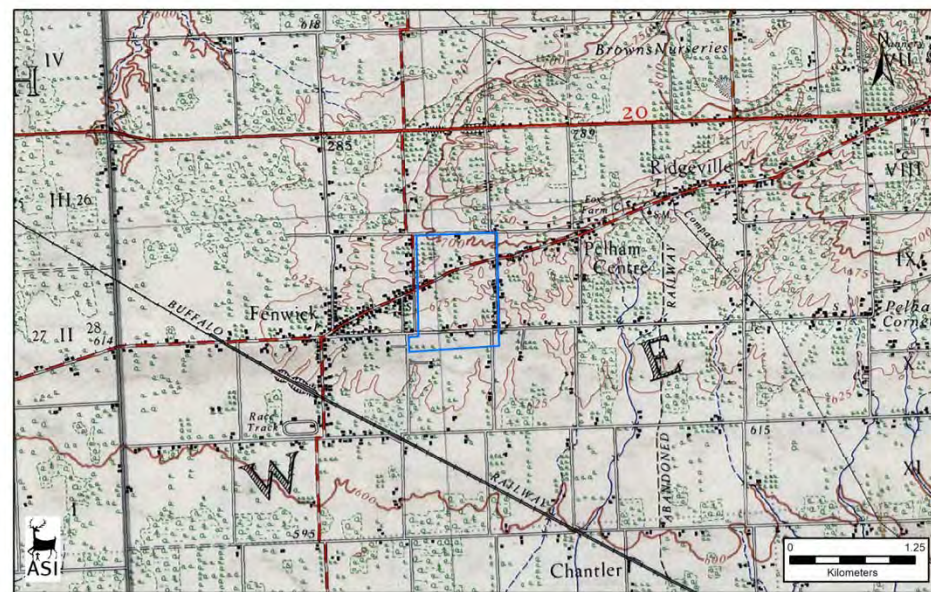


Figure 6: 1938 National Topographic Survey (NTS)
 (Source: NTS 1938)



Figure 7: 1954 Aerial Photograph
 (Source: Hunting Survey Corporation 1954)

4.0 DATA COLLECTION RESULTS

In order to make a preliminary identification of existing built heritage resources and cultural heritage landscapes within the study area and to collect any relevant information, the Town of Pelham's Municipal Register of Cultural Heritage Resources was consulted, including:

- Town of Pelham Municipal Register of Cultural Heritage Resources (2016)
- Niagara Region GIS Navigator, Heritage Designation layer (2016) (<https://maps-beta.niagararegion.ca/Navigator/>)
- Pelham Historical Society Collection and Archives, at the Fenwick Branch of the Pelham Public Library, in consultation with the Society Archivist, Mary Lamb, including the Pnyx Historical Calendar Collection (<http://vitacollections.ca/pelhamlocalhistory/results?q=pnyx&st=kw>)

Other resources consulted for the preliminary identification of cultural heritage resources within the study area included:

- The Ontario Heritage Trust's Ontario Heritage Plaque Guide, an online, searchable database of Ontario Heritage Plaques¹
- the Federal Heritage Buildings Review Office (FHBRO) [these properties are recognized under the Treasury Board Policy on the Management of Real Property (TBPMRP)]²
- Park's Canada's *Canada's Historic Places* website: available online³, the searchable register provides information on historic places recognized for their heritage value at the local, provincial, territorial and national levels.
- Parks Canada website (national historic sites)⁴

In addition, municipal staff at the Town of Pelham was contacted to gather any relevant information regarding cultural heritage resources and concerns within the study area (by email communication, May 24th 2017).

A field review was undertaken by ASI on July 5, 2017 to document the existing conditions of the study area. The field review was preceded by a review of available, current, and historical aerial photographs and maps (including online sources such as Bing and Google maps). The existing conditions of the study area are described below. Identified cultural heritage resources are discussed in Table 2 and Table 3 and mapped in Figure 12 of this report.

4.1 East Fenwick Secondary Plan – Existing Conditions

The study area includes that area within the urban area boundary of Fenwick that is bounded by Memorial Drive to the north, Balfour Street to the west, land on the south side of Welland Road to a depth of approximately 120m to the south and Cream Street to the east and comprises approximately 95 ha (235 acres). Canboro Road bisects the study area and is identified as an arterial road and Welland Road along the south boundary is considered to be a collector road with all the other streets being local roads. As a

¹ <http://www.heritagetrust.on.ca/Resources-and-Learning/Online-Plaque-Guide.aspx> [Accessed 24 October 2016]

² <http://www.pc.gc.ca/progs/beefp-fhbro/roles/beefp-fhbro.aspx> [Accessed 24 October 2016]

³ <http://www.historicplaces.ca/en/home-accueil.aspx> (accessed 24 October 2016).

⁴ <http://www.pc.gc.ca/eng/progs/lhn-nhs/index.aspx> [Accessed 24 October 2016]



part of the East Fenwick Secondary Plan, new zoning and land-use designations will be proposed for the area. The area is historically predominantly rural agricultural, and this agricultural character is still reflected in the existing conditions.

4.1.1 Character Areas

Prior to the identification of potentially significant cultural heritage landscapes, lands within the study area were classified into character areas in order to understand the predominant character and development patterns of the landscapes and streetscapes of East Fenwick (See Figure 8 in Appendix A). Identification of a character area does not necessarily indicate or confirm the presence of significant built heritage resources or cultural heritage landscapes.

Character areas share a consistent pattern of:

- Built form, such as architectural style, building height, building age, or setback;
- Streetscape features, road width, paving type, shoulders, ditches, sidewalks, streetlights;
- Landscape features, such as lot size, vegetation, topography, tree species, tree lines;
- And/or development pattern or history.

Seven distinct character areas have been identified:

- Traditional Agricultural Character Area
- Single Family Residential Infill Character Area
- Sunset Drive Development Character Area
- Alder Crescent Character Area
- Canboro Road Character Area
- Canboro Road Historic Scenic Road
- Memorial Drive Historic Scenic Road

Traditional Agricultural Character Area

This area includes properties in the interior of the study area, including the majority of properties to the north of Welland Road, as well as select properties to the north of Canboro Road, and to the south of Memorial Drive. These lands have traditionally been used as agricultural fields, or have been associated with former farms or early agricultural development, and currently retain this use or evidence of this use. The character attributes of this area include open, rolling and hilly terrain, the Fonthill Kame, agricultural fields, treelines, historic fence lines, black walnut trees and wild grapes. This area includes lands identified as being a part of the Canboro Road Provincially Significant Wetland. Part of the properties identified as BHR 2, BHR 8 and BHR 21 are located within this character area. This area has not been identified as a cultural heritage landscape (CHL).

Single Family Residential Infill Character Area

This area includes Cream Street and Welland Road as well as properties immediately adjacent to Cream Street, Memorial Drive and the south side of Welland Road. These lands have been developed gradually over time, and include primarily single family residences built between the nineteenth century and the present, with the majority of the buildings being built after 1964. They maintain a consistent, but varied setback, including front yards and driveways. These homes are typically 1-3 storeys, and include a variety



of architectural styles, consistent with the development pattern of gradual infill. Almost all of the residences face the street. Cream Street, Memorial Drive, and to a lesser extent, Welland Road, maintain a more rural streetscape, including little or no shoulder, few ditches, no sidewalks, no curbs, and relatively slow, low traffic levels. Properties along Memorial Drive and Cream Street which are located adjacent to or within the Canboro Road Provincially Significant Wetland incorporate the natural landscape and features. The properties identified as BHRs 4, 5, 14, 15, 16, 18, 19 and 20 are located within this character area. This area has not been identified as a cultural heritage landscape (CHL).

Sunset Drive Development Character Area

This area includes Sunset Drive, as well as property immediately adjacent to Sunset Drive. Sunset drive was surveyed as a road circa 1960, and the agricultural lands on either site were subdivided and sold as individual lots for residential development. The majority of the residences within the study area were built between 1960 and 1980, and consist of architectural styles typical of this era. Homes are typically 1-2 stories, with large lot sizes, varied setbacks, mature trees and vegetation, driveways and front yards. Sunset Drive is a narrow road with no painted lines, shoulder, sidewalks, or ditches, with street lights and above ground power lines. Sunset drive connects Canboro Road and Memorial Drive, however, it is not a straight road, but curves gradually, back and forth. No BHRs have been identified within this character area. This area has not been identified as a cultural heritage landscape (CHL).

Alder Crescent Character Area

This area includes Adler Crescent, as well as well as property immediately adjacent to Adler Crescent. Adler Crescent is a recent contemporary sub division, built circa 2000, consisting of a crescent shaped roadway, large estate lots, and very large 1-4 storey single family residences. No BHRs have been identified within this character area. This area has not been identified as a cultural heritage landscape (CHL).

Canboro Road Character Area

This area includes properties immediately adjacent to Canboro Road. Canboro Road is a former Indigenous trail, which was used extensively for early travel and settlement. A clear pattern of development appears in topographic and air photos, growing out of Fenwick along Canboro Road, towards the former historic settlement area of Pelham Centre. These lands were developed gradually over time, and include primarily single family residences built between the nineteenth century and the present, with the majority of the buildings being built before 1973. They maintain a consistent, but varied setback, including front yards and driveways. These homes are typically 1-3 storeys, and include a variety of architectural styles, consistent with the development pattern of gradual infill. All of the residences face the street. A collection of properties identified as built heritage resources have been identified within this character area, including BHRs 1, 2, and 6-13 are located within this character area. This area has not been identified as a cultural heritage landscape (CHL), although it is located directly adjacent to the Canboro Road Historic Scenic Road (CHL 2). Built heritage resources along Canboro Road contribute to the heritage character of CHL 2.

Canboro Road Historic Scenic Road

This area includes the Canboro Road corridor and runs diagonally through the centre of East Fenwick. Canboro Road is a former Indigenous trail, which was used extensively for early travel and settlement. The Canboro Road Corridor has been identified as an important historic scenic route and grouping of heritage resources in the Town of Pelham Heritage Master Plan (2012). Character attributes include its



diagonal alignment cutting across the concession grid, its connection to component communities, including Fenwick and Pelham Centre, the historic rural character of the street, including little or no shoulder, few ditches, few sidewalks, no curbs, and relatively slow, low traffic levels, and the mature roadside vegetation. Canboro Road benefits from slow traffic speeds and low-medium volumes of vehicular traffic. The large roadside trees and adjacent buildings provide the elements of a scenic drive and set it apart from roads that serve as traffic arteries. A collection of properties identified as built heritage resources have been identified adjacent to this character area, including BHRs 1, 2, and 6-13 are located within this character area. This area has been identified as a cultural heritage landscape (CHL 1) and retains cultural heritage value.

Memorial Drive Historic Scenic Road

This area includes the Memorial Drive corridor, a historically surveyed road. The area consists of a straight, undivided paved road with an east-west orientation. The topography is fairly flat with some gently rolling hills. The historic rural character of the street includes little or no shoulder, few ditches, no sidewalks, no curbs, and relatively slow, low traffic levels, and the mature roadside vegetation. The roadway is lined with hydro poles and mature trees. Memorial Drive is located adjacent to the Canboro Road Provincially Significant Wetland, and this is reflected in the natural landscape and mature vegetation adjacent to the road. Memorial Drive benefits from slow traffic speeds and low-medium volumes of vehicular traffic. This creates an environment that facilitates pedestrian and cycling activity, which contributes to the character of the area. Memorial drive also acts as the transition road between the urban boundary and the rural areas to the north. A gradual, and cohesive transition currently exists between these two areas. No BHRs have been identified within or adjacent to this character area. This area has been identified as a cultural heritage landscape (CHL 1) and retains cultural heritage value.

4.1.2 Summary of Public Consultation Results

On June 21, 2017, SGL associates, led by Ute Maya-Giambattista, conducted a presentation and workshop to the St. Ann Catholic Elementary School regarding the East Fenwick Secondary Plan. The aim of the workshop was to obtain student input regarding the key features that make Fenwick a great community and the desired development features for Fenwick's growth. The students identified preserving the community's greenspaces, wildlife and trail-system, maintaining the community's small-town and agricultural feel, creating safe sidewalks and bike paths and creating more parks for the community as important.

On June 22, 2017, Town staff members and the consulting team assembled at the Village of Fenwick's Fire Station 2 to conduct a Visioning Workshop regarding the East Fenwick Secondary Plan. The workshop aimed to obtain public input regarding the type of development most appropriate for the Village, and the types of community features most desired by the residents. In general, members of the public were concerned with the implications of the Secondary Plan with regards to development in the community, including concern for environmental preservation of trees, wildlife, the need for maintaining Fenwick's small-town feel and the impacts of projected traffic counts based on Provincial minimum density requirements, and forecasted population growth as a result of development in the Secondary Plan study area.

The results of these public consultation sessions were considered as part of the cultural heritage analysis presented in this report, and through identification of the pedestrian-friendly historic rural routes, historic settlement patterns, and the contribution of natural heritage to the development and character of East Fenwick.



4.2 East Fenwick Secondary Plan – Identified Cultural Heritage Resources

Based on the results of the background research, character area analysis, and field review, there are 23 cultural heritage resources within and adjacent to the study area, including: two cultural heritage landscapes, three residential/farmscape properties and 18 residential properties. See Table 2 for a summary of built heritage resources and cultural heritage landscapes and Table 4 in Appendix B for a detailed description of these identified resources. See Figure 9 in Appendix B for detailed mapping. Built heritage resources are mapped to the property parcel, however, further analysis is required to determine the boundaries of each resource’s significant heritage attributes.

Table 2: Summary of Built Heritage Resources (BHR) and Cultural Heritage Landscapes (CHL) in the Study Area

Feature ID	Location	Recognition	Description/Comments
CHL 1	Memorial Drive, between Balfour Street and Cream Street	Identified during field review	Approximately 815 m long portion of a straight, undivided paved road with an east-west orientation. Located in a transition area between urban and rural boundaries. Lined with mature trees and hydro poles, the character of the adjacent landscape ranges from residential houses, to farm fields, to brush and woodland.
CHL 2	Canboro Road, between Balfour Street and Cream Street	Identified in the Pelham Heritage Master Plan, and in section B2.2.7 of the Official Plan.	Approximately 860 m long portion of a two-lane paved road with a diagonal orientation running southwest to northeast. The road cuts through a rural agricultural area. Lined with mature trees and hydro poles, the majority of this road section is flanked by residential houses, but there are also fields and stretches of open green space.
BHR 1	695 Canboro Rd.	Listed	2.5-storey red brick church building built in 1886. Converted into apartments in the mid twentieth century.
BHR 2	655 Canboro Rd.	Listed	Two-storey, red brick Gothic Revival-style residential building built in 18712. A garage is located to the west of the house.
BHR 3	704 Canboro Rd.	Listed	Two-storey red brick Edwardian-style school building built in 1927 with large mid-century brick additions. Converted into apartments in the 1970s. Property contains a large parking lot.
BHR 4	1159 Cream St.	Listed	Two-storey Folk Victorian-style frame residential building clad in siding, built in 1870 as the home of John Crow.
BHR 5	606 Canboro Rd.	Identified during field review	1.5-storey L-shaped stucco residential cottage with a side addition.
BHR 6	607 Canboro Rd.	Identified during field review	1.5-storey frame residential building with board and batten siding. Original rectangular house has an addition similar in size and materials.
BHR 7	615 Canboro Rd.	Identified during field review	One-storey frame residential cottage clad in board and batten siding with a rear addition. A garage is located east of the house.
BHR 8	645 Canboro Rd.	Identified during field review	Two-storey frame residential building clad in siding. The property contains outbuildings and farm fields.
BHR 9	668 Canboro Rd.	Identified during field review	1.5-storey frame residential building clad in vinyl siding. An outbuilding is located behind the house.
BHR 10	675 Canboro Rd.	Identified during field review	1.5-storey frame residential building clad in aluminum siding with a rear addition. Outbuildings are located behind the house.

Table 2: Summary of Built Heritage Resources (BHR) and Cultural Heritage Landscapes (CHL) in the Study Area

Feature ID	Location	Recognition	Description/Comments
BHR 11	687 Canboro Rd.	Identified during field review	Two-storey frame residential building clad in vinyl siding. A garage has been added to the west side of the house.
BHR 12	688 Canboro Rd.	Identified during field review	Two-storey frame residential building clad in siding. A garage is located to the east of the house.
BHR 13	691 Canboro Rd.	Identified during field review	1.5-storey frame residential building clad in vinyl siding, with a rear addition. A large barn is located east of the house.
BHR 14	1108 Cream St.	Identified during field review	1.5-storey frame residential cottage built c. 1879. Clad in siding, with a large single storey addition on the south side.
BHR 15	1118 Cream St.	Identified during field review	1.5-storey Victorian frame residential building clad in siding.
BHR 16	1128 Cream St.	Identified during field review	1.5-storey frame residential building clad in siding.
BHR 17	1162 Cream St.	Identified during field review	1.5-storey frame residential building clad in siding with a brick-clad addition on the north side
BHR 18	578 Welland Rd.	Identified during field review	2-storey frame residential building clad in siding. A garage is located to the east of the house.
BHR 19	630 Welland Rd.	Identified during field review	1.5-storey frame residential building clad in siding, with a single-storey extension on the west side. A garage is located to the east of the house.
BHR 20	646 Welland Rd.	Identified during field review	1.5-storey frame residential building clad in vinyl siding, with a large addition at the rear.
BHR 21	663 Welland Rd.	Identified during field review	One-storey, frame residential building clad in vinyl siding. A garage is located northeast of the house.

4.3 East Fenwick Secondary Plan – Preliminary Impact Analysis

Development activities have the potential to affect cultural heritage resources in a variety of ways, and as such, appropriate mitigation measures need to be considered prior to the development of preferred land uses. Appropriate mitigation measures will be developed upon the selection of a preferred alternative for the subject secondary plan, including land uses, development, road and pedestrian realm improvements, and other recommendations resulting from the secondary plan process. A preliminary heritage impact analysis has been included below in Table 3 to inform the secondary plan process.

Table 3: Potential Impacts of Secondary Plan Land Uses on Identified CHLs and BHRs

Feature ID	Description	Potential Impact	Mitigation Approaches
CHL 1	Memorial Drive CHL	<ul style="list-style-type: none"> • Alteration or removal of heritage attributes due to: <ul style="list-style-type: none"> ○ Road widening ○ Loss of mature vegetation ○ Increased traffic volumes ○ Pedestrian realm improvements ○ Road improvements ○ Incompatible development adjacent to resource ○ Loss of gradual transition from urban boundary to rural area 	<ul style="list-style-type: none"> • Study for recognition of Memorial Drive as a Heritage Route under D4.2.2.6 of the OP is recommended. • Consideration of alternative road and pedestrian realm improvement approaches to conserve and enhance the corridor. • Consideration of alternative development and land use approaches that conserve and enhance the landscape and built form character of adjacent properties, and encourage a gradual transition from the urban boundary to the rural areas to the north. • Heritage impact analysis of proposed land use plan, once a preferred alternative has been developed, with the development of

Table 3: Potential Impacts of Secondary Plan Land Uses on Identified CHLs and BHRs			
Feature ID	Description	Potential Impact	Mitigation Approaches
CHL 2	Canboro Road CHL	<ul style="list-style-type: none"> • Alteration or removal of heritage attributes due to: <ul style="list-style-type: none"> ○ Road widening ○ Loss of mature vegetation ○ Increased traffic volumes ○ Pedestrian realm improvements ○ Road improvements ○ Incompatible development adjacent to resource 	specific mitigation measures. <ul style="list-style-type: none"> • Study for designation or recognition of the Canboro Road Corridor as a Cultural Heritage Landscape, through Part V of the OHA or through an OPA is recommended. • Consideration of alternative road and pedestrian realm improvement approaches to conserve and enhance the corridor. • Consideration of alternative development and land use approaches that conserve and enhance the landscape and built form character of adjacent properties. • Heritage impact analysis of proposed land use plan, once a preferred alternative has been developed, with the development of specific mitigation measures.
BHR 2, 8, 16 and 20	655 Canboro Rd., 645 Canboro Rd., 1128 Cream St., and 646 Welland Rd.	<ul style="list-style-type: none"> • Alteration or demolition of property, related to future development and road widening and improvements and/or pedestrian realm improvements 	<ul style="list-style-type: none"> • Properties contain heritage features that are good candidates for conservation. Based on a review of the East Fenwick Secondary Plan Area Conceptual Tertiary Plan, prepared by Upper Canada Consultants in 2010, and consideration of properties then shown to be owned by a developer's group, it is expected that these properties may be subject to impacts. • Heritage Impact Assessments should be completed for the subject properties, to confirm the cultural heritage value of the property, and assess the impacts of the proposed work. • Heritage impact analysis of proposed land use plan should be completed once a preferred alternative has been developed, with the development of specific mitigation measures.
BHRs 1, 5-7, 9-15, 17-19, and 21	Built Heritage Resources	<ul style="list-style-type: none"> • Alteration of properties, related to road widening and improvements and/or pedestrian realm improvements 	<ul style="list-style-type: none"> • Properties contain heritage features that may, upon further investigation, warrant conservation. Based on a review of the East Fenwick Secondary Plan Area Conceptual Tertiary Plan, prepared by Upper Canada Consultants in 2010, and consideration of properties then shown to be owned by a developer's group, impacts to the subject properties are not anticipated by proposed land use changes. • Heritage impact analysis of proposed land use plan should be completed once a preferred alternative has been developed, with the development of specific mitigation measures. • Should future development propose alteration or demolition of the identified resources, a Heritage Impact Assessment should be completed, to confirm the cultural heritage value of the property, and assess the impacts of the proposed work.
BHR 3	704 Canboro Rd	<ul style="list-style-type: none"> • Outside of study area, no anticipated impacts 	<ul style="list-style-type: none"> • No further work required for the Secondary Plan.

Feature ID	Description	Potential Impact	Mitigation Approaches
			<ul style="list-style-type: none"> Should future development propose alteration or demolition of the identified resource, a Heritage Impact Assessment should be completed, to confirm the cultural heritage value of the property, and assess the impacts of the proposed work.
BHR 4	1159 Cream St.	<ul style="list-style-type: none"> Alteration to property related to road widening and improvements and/or pedestrian realm improvements 	<ul style="list-style-type: none"> Recognition through Part IV designation under Ontario Heritage Act is recommended. Should future development propose alteration or demolition of the identified resource, a Heritage Impact Assessment should be completed, to confirm the cultural heritage value of the property, and assess the impacts of the proposed work.

5.0 CONCLUSIONS

The results of the background historical research and a review of secondary source material, including historical mapping revealed a study area with a rural land use history dating back to the early nineteenth century. The field review and character area analysis confirmed that this area retains a number of nineteenth- and twentieth-century cultural heritage resources. The following provides a summary of the assessment results:

Key Findings

- A total of 23 cultural heritage resources were identified within and/or adjacent to the East Fenwick Secondary Plan study area;
- These resources include two cultural heritage landscapes, three residential/farmscape properties and 18 residential properties.
- Identified cultural heritage resources are historically, architecturally, and contextually significant rural and agricultural properties and landscapes, which have emerged from their physiographic and natural heritage contextual setting, and contribute to consistent land use patterns within the East Fenwick Secondary Plan study area.

Preliminary Impact Assessment

All 23 cultural heritage resources identified within the study area were assessed for potential impacts, and the following provides a summary of impact screening results:

- BHR 3 is located outside of the study area, and no impacts are anticipated, accordingly, no further work for this property is required for this property as a part of the East Fenwick Secondary Plan.
- BHR 4 is located outside of the study area, but may be altered as a result of road widening and/or improvements and/or pedestrian realm improvements. Designation under Part IV of the Ontario Heritage Act is recommended for this property. The heritage impact of the proposed land use plan, resulting from the secondary plan process, should be assessed once a preferred alternative



has been developed, including the development of specific mitigation measures. A Heritage Impact Assessment should be completed should any additional alterations or demolition be proposed for the property.

- BHRs 2, 8, 16 and 20 contain heritage features that are good candidates for conservation. Based on a review of the East Fenwick Secondary Plan Area Conceptual Tertiary Plan, prepared by Upper Canada Consultants in 2010, and consideration of properties then shown to be owned by a developer's group, it is expected that these properties may be subject to impacts, through alteration or possible demolition, as a result of future development or land use changes, road widening and/or improvements and/or pedestrian realm improvements. A Heritage Impact Assessment should be completed for the subject properties. The heritage impact of the proposed land use plan resulting from the secondary plan process should be completed, once a preferred alternative has been developed, with the development of specific mitigation measures.
- BHRs 1, 5-7, 9-15, 17-19, and 21 may be altered as a result of road widening and/or improvements and/or pedestrian realm improvements. The heritage impact of the proposed land use plan resulting from the secondary plan process should be assessed, once a preferred alternative has been developed, including the development of specific mitigation measures. A Heritage Impact Assessment should be completed to confirm the cultural heritage value of these properties should any additional alterations or demolition be proposed for the property.
- CHL 2 is important historic rural route and former Indigenous trail, which has been previously identified in the Pelham Heritage Master Plan, and in Section B2.2.7 of the Official Plan. CHL 2 maintains a strong connection to the historic settlement patterns of East Fenwick and to the built heritage resources found along the road. CHL 2 is recommended to be studied for designation or recognition as a Cultural Heritage Landscape, through Part V of the Ontario Heritage Act or through an Official Plan Amendment. CHL2 may be impacted by the alteration or removal of heritage attributes due to: Road widening, loss of mature vegetation, increased traffic volumes or speeds, pedestrian realm improvements, road improvements, and incompatible development adjacent to resource. The heritage impact of the proposed land use plan resulting from the secondary plan process should be assessed, once a preferred alternative has been developed, including the development of specific mitigation measures. Alternative road and pedestrian realm improvement approaches to conserve and enhance the corridor and alternative development and land use approaches that conserve and enhance the landscape and built form character of adjacent properties should be considered for CHL 2 as a part of the secondary plan process.
- CHL 1 is an important historic rural route, characterized by little or no shoulder, no formal ditches, no sidewalks, no curbs, relatively slow, low traffic levels, and mature roadside vegetation. These attributes facilitate pedestrian and cycling activity. CHL 1 acts as a boundary road between the urban and rural areas of East Fenwick. CHL 1 is recommended to be studied for recognition as a Heritage Route under D4.2.2.6 of the Official Plan. CHL 1 may be impacted by the alteration or removal of heritage attributes due to: Road widening, loss of mature vegetation, increased traffic volumes or speeds, pedestrian realm improvements, road improvements, incompatible development adjacent to resource and the loss of the gradual transition from urban boundary to rural area. The heritage impact of the proposed land use plan resulting from the secondary plan process should be assessed once a preferred alternative has been developed, including the development of specific mitigation measures. Alternative road and pedestrian realm improvement approaches to conserve and enhance the corridor and alternative development and land use approaches that conserve and enhance the landscape and encourage a gradual transition



from the urban boundary to the rural areas to the north should be considered for CHL 1, as a part of the secondary plan process.

6.0 RECOMMENDATIONS

The background research, data collection, and field review conducted for the study area determined that there are two cultural heritage landscapes, three residential/farmscape properties and 18 residential properties within the East Fenwick Secondary Plan study area. These cultural heritage resources combine to create a study area with a rural land use history dating back to the mid-nineteenth century, which has emerged from the unique physiographic and natural heritage contextual setting. As a result of the research and analysis found in this report, the identified cultural heritage resources are strong candidates for conservation and integration into future land uses in the secondary plan area, or should be subject to cultural heritage impact statements during subsequent development planning applications.

As part of the development of policies for the East Fenwick Secondary Plan, the following mitigation measures and/or alternative development approaches should be incorporated to reduce the potential for adverse impacts to the cultural heritage resources in the area. Common mitigation protocols may include, but are not limited to, the following and are suitable for consideration and application for minimizing impacts on cultural heritage resources:

- Avoidance and mitigation to allow development to proceed while retaining the cultural heritage resources in situ and intact;
- Adaptive re-use of a built heritage structure or cultural heritage resources;
- Alternative development approaches to conserve and enhance a significant heritage resource;
- Avoidance protocols to isolating development and land alterations to minimize impacts on significant built and natural features and vistas;
- Historical commemoration of the cultural heritage of a property/structure/area, historical commemoration by way of interpretive plaques;
- Documentation and salvage including the relocation of a structure or (as a last resort) the salvaging of its architectural components may be considered;
- Architectural design guidelines for buildings on adjacent and nearby lots to help integrate and harmonize mass, setback, setting, and materials;
- Limiting height and density of buildings on adjacent and nearby lots;
- Ensuring compatible lot patterns, situating parks and storm water ponds near a heritage resource;
- Vegetation buffer zones, tree planting, site plan control and other planning mechanisms;
- Allowing only compatible infill and additions;
- Preparation of cultural heritage impact assessments for all developments affecting a cultural heritage resource;
- Preparation of conservation, restoration and adaptive reuse plans as necessary;
- Heritage Designation, Heritage Conservation Easement; and
- Preparation of security plan and/or letter of credit to help ensure security and protection of heritage resources.

Based on the results of the assessment, the following recommendations have been developed:

1. A total of 23 cultural heritage resources were identified within and/or adjacent to the East Fenwick Secondary Plan study area, which include two cultural heritage landscapes, three residential/farmscape properties and 18 residential properties. The East Fenwick Secondary Plan



should incorporate policies that ensure the long-term viability and presence of significant built heritage resources and cultural heritage landscapes.

2. BHR 4 should be considered for designation under Part IV of the Ontario Heritage Act.
3. CHL 2 is important historic rural route and former Indigenous trail, which has been previously identified in the Pelham Heritage Master Plan, and in Section B2.2.7 of the Official Plan. CHL 2 maintains a strong connection to the historic settlement patterns of East Fenwick and to the built heritage resources found along the road. CHL 2 should be studied for designation or recognition as a Cultural Heritage Landscape, under Part V of the Ontario Heritage Act, or through an Official Plan Amendment.
4. CHL 1 is an important historic rural route, characterized by little or no shoulder, no formal ditches, no sidewalks, no curbs, relatively slow, low traffic levels, and mature roadside vegetation. These attributes facilitate pedestrian and cycling activity. CHL 1 acts as a boundary road between the urban and rural areas of East Fenwick. CHL1 should be studied for recognition as a Heritage Route under D4.2.2.6 of the Official Plan.
5. Alternative road and pedestrian realm improvement approaches to conserve and enhance the road corridors and alternative development and land use approaches that conserve and enhance the landscape and built form character of adjacent properties should be considered for CHL 1 and CHL 2 as a part of the secondary plan process. A gradual transition from the urban boundary to the rural areas to the north should be considered for CHL 1.
6. BHRs 2, 8, 16 and 20 contain heritage features that are good candidates for conservation. Based on a review of the East Fenwick Secondary Plan Area Conceptual Tertiary Plan, prepared by Upper Canada Consultants in 2010, and consideration of properties then shown to be owned by a developer's group, it is expected that these properties may be subject to impacts as a result of future development or land use changes. A Heritage Impact Assessment should be completed for the subject properties.
7. CHLs 1-2 and BHRs 1- 2, 4-21 may be altered as a result of changes in land use, future development, road widening and/or improvements and pedestrian realm improvements. Upon the completion of a proposed land use plan resulting from the secondary plan process, the heritage impacts should be assessed, including the development of specific mitigation measures.

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Appendix A: *Major Participating Landowners*. Copy on file with ASI.



APPENDIX A: Character Areas Located Within the East Fenwick Secondary Plan Study Area

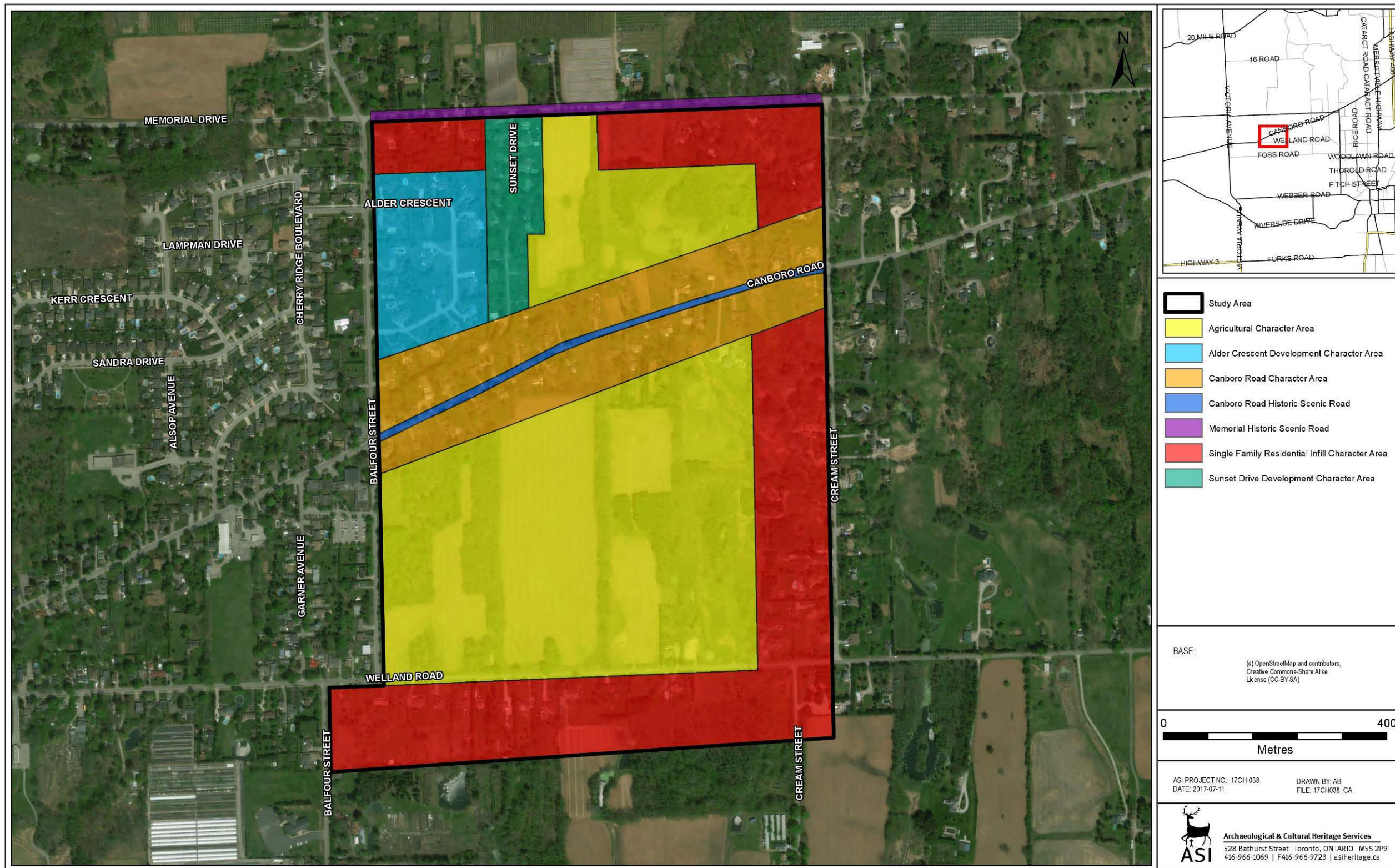


Figure 8: Location of identified Character Areas located within the East Fenwick Secondary Plan study area.

APPENDIX B: Identified Cultural Heritage Resources (CHR) Located Within the East Fenwick Secondary Plan Study Area

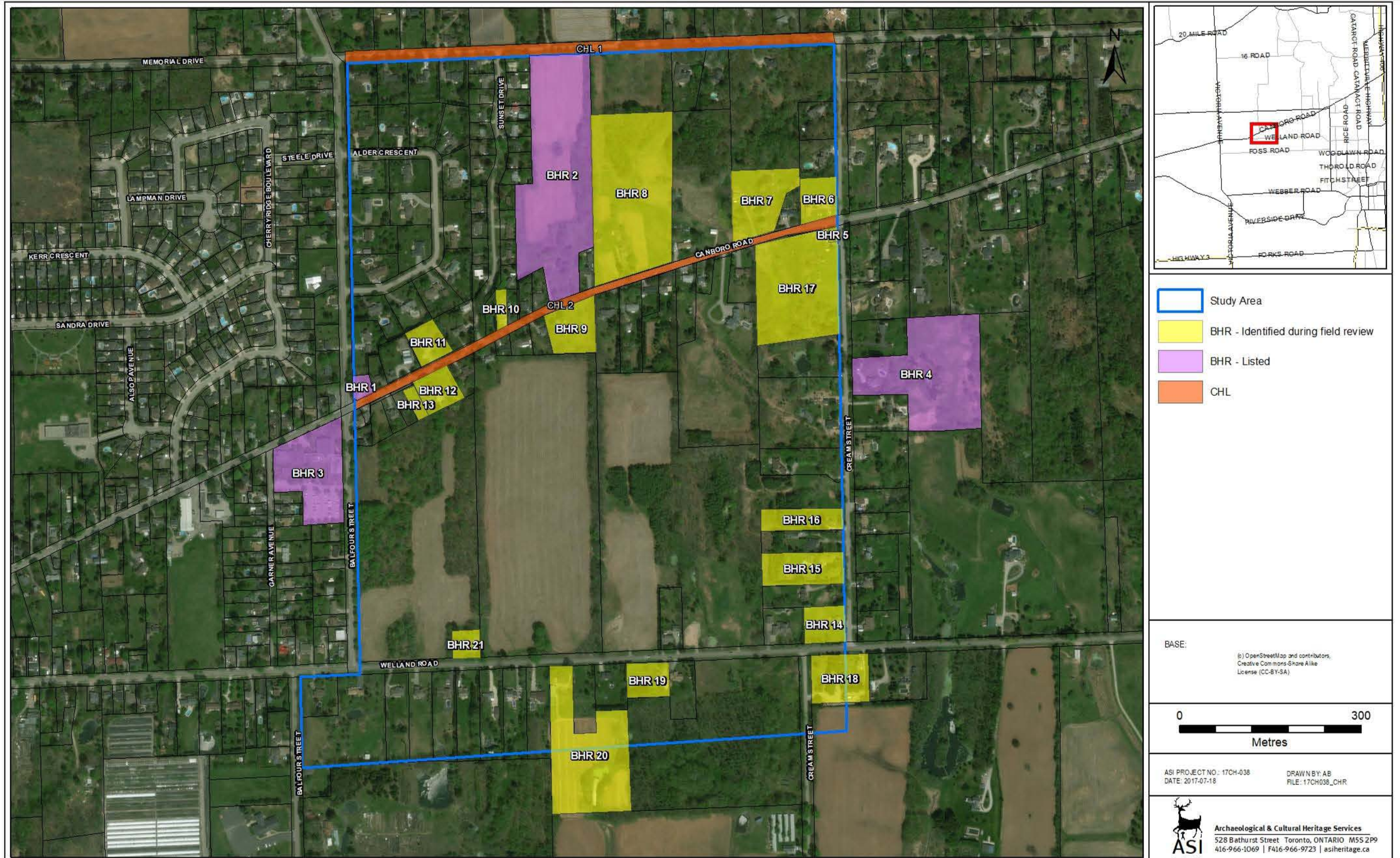


Figure 9: Location of identified Cultural Heritage Resources (CHR) located within the East Fenwick Secondary Plan study area.





Table 4 Detailed description of built heritage resources (BHR) and cultural heritage landscapes (CHL) in the study area					
Feature ID	Address	Heritage Status	Resource Type	Description/Comments	Photograph(s)
CHL 1	Memorial Drive, between Balfour Street and Cream Street	Identified during field review	Cultural Heritage Landscape; Roadway and streetscape	<p>Design: This portion (approximately 815 m) of Memorial Drive consists of a straight, undivided paved road and an east-west orientation. The topography is fairly flat with some gently rolling hills. There are no shoulders, curbs, sidewalks or formal ditches. The roadway is lined with hydro poles and mature trees. The character of the surrounding landscape along this section of road can be divided into three sections. The section west of Cream St. contains brush and woodland growing up to the edges of the road, with large homes hidden from view on large lots. The central section contains farm fields and some houses. The section east of Balfour St. contains a more dense concentration of houses. Most of the houses appear to have been built in the later twentieth century.</p> <p>History: A review of historic mapping reveals that Memorial Drive was an historically surveyed road, with development along it occurring slowly from the nineteenth century through to the mid twentieth century. The age of the homes along the road would indicate that the majority of the residential development occurred in the mid-to-late twentieth century.</p> <p>Context: Vehicle traffic along this section of road is low. Pedestrian traffic was also noted during the field visit. It is located in a transition area between urban and rural boundaries, with an expanse of agricultural lands to the north and a twenty-first-century subdivision located just southwest of the intersection of Memorial Drive and Balfour Street. This section of Memorial Drive also intersects with a small subdivision established in the 1970s on Sunset Drive.</p>	 
CHL 2	Canboro Road, between Balfour Street and Cream Street	Identified during field review. The Canboro Road Corridor is also identified as a scenic drive in the Town of Pelham's Municipal Heritage Master Plan. It is also identified as a rural promenade in the Town of Pelham's Official Plan.	Cultural Heritage Landscape; Roadway and streetscape	<p>Design: This portion (approximately 860 m) of Canboro Road consists of a two-lane, divided paved road with a diagonal orientation running southwest to northeast. The topography is fairly flat with some gently rolling hills. There is a narrow, paved shoulder, with no curbs and no formal ditches. A narrow sidewalk is located on the north side of the road for approximately 200 metres in the eastern-most part of the study area. The roadway is lined with hydro poles and mature trees. The majority of this road section is flanked by residential houses, but there are also fields and stretches of open green space. The houses along the road are a mix of ages and have varying setbacks. There are a number of nineteenth century farmhouses.</p> <p>History: The Canboro Road was the route along which the first settlements in Pelham were established. Historic mapping suggests that the Canboro Road was not actually surveyed and improved as a road until the 1840s. Oral narratives identify Canboro Road as an old Indigenous trail, along with Lundy's Lane (to the east) and the Talbot Road (to the west), which both connect with the Canboro Road. Canboro Road runs diagonally through the historically surveyed roads that together form the historic road network which together formed the early infrastructure of Fenwick, and connected the area regionally. The mix of house ages indicates that development occurred slowly along Canboro Road.</p> <p>Context: This section of Canboro Road cuts through a rural agricultural area, with some of the best soil for fruit production in Ontario. A twenty-first-century subdivision is located just northwest of the corner of Canboro Road and Balfour Street. The traffic speed is slow and vehicle volume is low-to-medium.</p>	 

Table 4 Detailed description of built heritage resources (BHR) and cultural heritage landscapes (CHL) in the study area




Feature ID	Address	Heritage Status	Resource Type	Description/Comments	Photograph(s)
BHR 1	695 Canboro Rd.	Listed	Church, converted into apartment building	<p>Design: A 2.5-storey red brick church building built in 1886 with a front gable roof. Two hip dormers have been added on the west side and an extension has been added on the east side. The symmetrical façade features a gabled projecting entryway and a door with sidelights and a flat transom topped with a lunette-shaped stained glass window. The first-storey has segmental windows with voussoirs. The upper storeys have semi-elliptical windows with voussoirs. The bays of the building are delineated by vertical bands of projecting brick and the frieze features decorative brickwork. A garage is located behind the building to the west. The property landscape consists mostly of a parking lot, with two exits onto Canboro Rd.</p> <p>History: This property is identified as belonging to “E.M.” in the 1862 Tremaine’s Map. R. Farr is identified as the property owner in the 1880 Historical Atlas map.</p> <p>Built in 1886 as the Bethany Episcopal Methodist Church. In 1902 the building was acquired by the Knox Presbyterian Church. On July 2nd 1947 the building was sold and then converted into an apartment building.</p> <p>Context: Located adjacent to the Canboro Rd. CHL at the northeast corner of Canboro Rd. and Balfour St., this property is in a transition area between urban and rural boundaries. A subdivision is located to the northwest, while the other surrounding areas are largely rural/agricultural.</p>	
BHR 2	655 Canboro Rd.	Listed	Residential, farmscape	<p>Design: Built in 1872. A two-storey, red brick Gothic Revival-style residential building with a front gable roof. A central front gable frames a single second-storey door. The first storey features two tall round-headed 2-over-2 sash windows and an off-centre entry, covered by a central porch supported by flattened columns. Bargeboard decorates the front gable and porch eaves. A garage is located to the west of the house. The property is set well back from the road and has a maintained lawn, mature plantings and an established entrance drive. The property also contains a large field behind the house.</p> <p>History: This property is identified as belonging to George Waters in the 1862 Tremaine’s Map. C. Reece is identified as the property owner in the 1880 Historical Atlas map.</p> <p>Context: Located adjacent to the Canboro Rd. CHL.</p>	
BHR 3	704 Canboro Rd.	Listed	School, converted into apartment building	<p>Design: A two-storey red brick Edwardian-style school building built in 1927. The symmetrical, stepped façade features a projecting centre bay with quoining. The frontispiece is topped by a pediment, with a semi-circular window with moulded trim on the second storey. The front door has a flat transom and moulded trim. The first and second storeys feature a moulded cornice and flat-headed windows. The building has long brick additions added in the mid-twentieth century on the west and south sides. The property features a maintained lawn with mature trees, a small parking lot at the front and a large rear parking lot.</p> <p>History: Built in 1927 as the Pelham Secondary School. Served as school from 1927-1974. It was then converted into an apartment building called the Canboro Gardens.</p> <p>This property is identified as belonging to James Garner in the 1862 Tremaine’s Map and in the 1880 Historical Atlas map. A building is identified as being in this location in the 1938 topographic map.</p> <p>Context: Located at the southwest corner of Canboro Rd. and Balfour St., this property is in a transition area between urban and rural boundaries. A subdivision is located to the northwest, while the other surrounding areas are largely rural/agricultural.</p>	




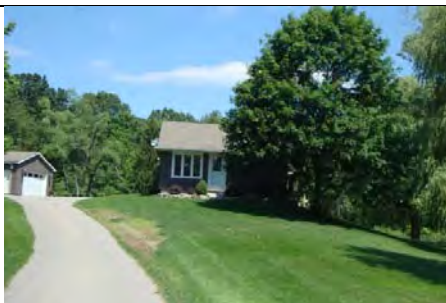
Table 4 Detailed description of built heritage resources (BHR) and cultural heritage landscapes (CHL) in the study area					
Feature ID	Address	Heritage Status	Resource Type	Description/Comments	Photograph(s)
BHR 4	1159 Cream St.	Listed	Residential	<p>Design: A two-storey Folk Victorian-style frame residential building clad in siding, built ca. 1870. The building features a rectangular floor plan and a symmetrical façade. The central entranceway has double doors framed by a portico and second storey balcony. The one-over-one sash windows have decorative trim. The cornice features brackets and a decorated soffit. A belvedere sits atop the hip roof. The property features a semi-circular driveway, a maintained lawn and numerous mature plantings.</p> <p>History: No property owner is identified in the 1862 Tremaine’s Map or in the 1880 Historical Atlas map, but a house is identified as being in this location in 1880.</p> <p>Johannes Groh (anglicized to John Crow) was a weaver who migrated from Pennsylvania in 1788 to settle in Pelham in with his wife and five children. His fifth child, Jacob Crow Sr., was a prosperous landowner who purchased this property in 1854, and it remained in the Crow family until 1914. The house (built ca. 1870) is recorded as the residence of Jacob’s nephew, William Crow, who lived there with his wife, Sarah Jane (Huntsman). Their son Alandis started canning produce on the property before establishing a canning factory at 410 Canboro Rd (later the site of Lindsay Lumber and now the site of the Ridgeville Post Office). William Crow’s cousin, John Bowman Crow (1821-1887), was a notable member of the Crow family who in 1859 was appointed clerk to the Township of Pelham. In 1883 he also took on the job of Township Treasurer. His son Judson C. Crow, who had been a schoolmaster, succeeded him in the post of clerk.</p> <p>Context: Located adjacent to the Canboro Rd. CHL.</p>	
BHR 5	606 Canboro Rd.	Identified during field review	Residential	<p>Design: A 1.5-storey, L-shaped stucco residential cottage with a side addition, likely built prior to 1880. The building has gable and hip roofs, an off-centre entrance, a brick chimney and flat-headed windows. It is located atop a berm at the corner of Cream St. and Canboro Rd., with a driveway accessible from both roads. The property has a maintained lawn and mature trees.</p> <p>History: No property owner is identified in the 1862 Tremaine’s Map. “Mrs. B.” is identified as the property owner in the 1880 Historical Atlas map. A house is identified as being in this location in 1880.</p> <p>Context: Located adjacent to the Canboro Rd. CHL., at the southwest corner of Canboro Rd. and Cream St.</p>	
BHR 6	607 Canboro Rd.	Identified during field review	Residential	<p>Design: A 1.5-storey frame residential building with board and batten siding, likely built prior to 1880. The building consists of two rectangular plans of similar size joined end to end with, with side gable roofs and flat-headed windows. The house is located atop a berm, with a maintained lawn, mature trees, and a driveway exiting onto Cream St.</p> <p>History: No property owner is identified in the 1862 Tremaine’s Map or the 1880 Historical Atlas map. A house is identified as being in this location in 1880.</p> <p>Context: Located adjacent to the Canboro Rd. CHL., at the northwest corner of Canboro Rd. and Cream St.</p>	
BHR 7	615 Canboro Rd.	Identified during field review	Residential	<p>Design: A one-storey, frame residential cottage clad in board and batten siding, likely built prior to 1880. The building features a rectangular floorplan and a side gable roof. A central entry is flanked by a wide flat-headed window on one side. A garage is located to the east of the house. The buildings are set well back from Canboro Rd. and accessed by a long driveway.</p> <p>History: No property owner is identified in the 1862 Tremaine’s Map or the 1880 Historical Atlas map. A house is identified as being in this location in 1880.</p> <p>Context: Located adjacent to the Canboro Rd. CHL.</p>	

Table 4 Detailed description of built heritage resources (BHR) and cultural heritage landscapes (CHL) in the study area






Feature ID	Address	Heritage Status	Resource Type	Description/Comments	Photograph(s)
BHR 8	645 Canboro Rd.	Identified during field review	Residential, farmscape	<p>Design: A two-storey, frame residential building with a hip roof and a rectangular floorplan, likely built prior to 1880. A front porch is topped with a balcony and two symmetrical flat-headed windows with shutters on the second-storey. Paired wooden brackets are featured along the cornice. Outbuildings are located to the north and northwest of the house. The property has a maintained lawn, mature trees, an established entrance drive and farm fields.</p> <p>History: This property is identified as belonging to “T.C.” in the 1862 Tremaine’s Map. No property owner is identified in the 1880 Historical Atlas map. A house is identified as being in this location in 1880.</p> <p>Context: Located adjacent to the Canboro Rd. CHL. The property appears to be an active farmscape, with fields located to the east and north of the house and possible agricultural buildings located behind the house.</p>	
BHR 9	668 Canboro Rd.	Identified during field review	Residential	<p>Design: A 1.5-storey, frame residential building with an L-shaped plan, likely built prior to 1880. The building has flat-headed windows and a cross gable roof. The entryway features a small porch with a bell-curved roof. Small setback. An outbuilding is located directly behind the house. The property has a maintained lawn, mature trees and an established entrance drive.</p> <p>History: This property is identified as belonging to J. Hicks in the 1862 Tremaine’s Map. George Cplar is identified as the property owner in the 1880 Historical Atlas map. A house and orchard are identified as being in this location in 1880.</p> <p>Context: Located adjacent to the Canboro Rd. CHL.</p>	
BHR 10	675 Canboro Rd.	Identified during field review	Residential	<p>Design: A 1.5-storey, frame residential building clad in aluminum siding with a cross gable roof and a rear addition, likely built between 1920 and 1938. The building has a front central gable, flat-headed windows, a central bay window and two brick chimneys. A garage and an outbuilding are located to the north of the house. The property has a maintained lawn, mature trees and an established entrance drive, with a small setback.</p> <p>History: This property is identified as belonging to George Waters in the 1862 Tremaine’s Map. C. Reece is identified as the property owner in the 1880 Historical Atlas map.</p> <p>Context: Located adjacent to the Canboro Rd. CHL.</p>	
BHR 11	687 Canboro Rd.	Identified during field review	Residential	<p>Design: A two-storey, frame residential building clad in vinyl siding with a side gable roof, rectangular floorplan and symmetrical façade, likely built between 1907 and 1920. The building features a central entranceway with a transom, sidelights and pilasters, flanked by flat-headed windows. A garage has been added to the west side of the house. The property has a maintained lawn, mature trees and an established entrance drive.</p> <p>History: This property is identified as belonging to “E.M.” in the 1862 Tremaine’s Map. R. Farr is identified as the property owner in the 1880 Historical Atlas map.</p> <p>Context: Located adjacent to the Canboro Rd. CHL.</p>	
BHR 12	688 Canboro Rd.	Identified during field review	Residential	<p>Design: A two-storey frame residential building clad in siding, likely built prior to 1880. The building has a verandah with a central pediment, decorative turned posts and decorative moulding along the verandah roofline. A garage is located to the east of the house. The property has a maintained lawn, mature trees and an established entrance drive.</p> <p>History: This property is identified as belonging to R. Farr in the 1862 Tremaine’s Map and in the 1880 Historical Atlas map. A house and an orchard are identified as being in this location in 1880.</p> <p>Context: Located adjacent to the Canboro Rd. CHL.</p>	









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Feature ID	Address	Heritage Status	Resource Type	Description/Comments	Photograph(s)
BHR 13	691 Canboro Rd.	Identified during field review	Residential	<p>Design: A 1.5-storey frame residential building clad in vinyl siding, with a rear addition, likely built between 1920 and 1938. The building has an L-shaped floorplan with cross gable roofs and stacked oriel windows. The entranceway is covered by a porch with a bell-curved roof, and a gable is centred over the porch. The porch features bargeboard and is supported by turned posts. The 2-over-2 sash windows are flat-headed. A large barn is located east of the house and is connected by the semi-circular driveway. The property has a maintained lawn and mature trees.</p> <p>History: This property is identified as belonging to R. Farr in the 1862 Tremaine’s Map and in the 1880 Historical Atlas map.</p> <p>Context: Located adjacent to the Canboro Rd. CHL.</p>	
BHR 14	1108 Cream St.	Identified during field review	Residential	<p>Design: A 1.5-storey frame residential cottage built c. 1879. Clad in siding, with a large single storey addition on the south side. The original cottage features a symmetrical facade with a side gable roof, a pointed central gable and flat-headed windows. The property has a maintained lawn, mature trees and an established entrance drive.</p> <p>History: Rason Cottage. This property is identified as belonging to J. Crow in the 1862 Tremaine’s Map. No property owner is identified in the 1880 Historical Atlas map. A house is identified as being in this location in 1880.</p> <p>Context: The property is located on Cream St., which is largely comprised of residential properties set back from the road within a rural/agricultural area.</p>	
BHR 15	1118 Cream St.	Identified during field review	Residential	<p>Design: A 1.5 storey, Victorian frame residential building clad in siding with an L-shaped floorplan, likely built prior to 1880. The building has cross gable roofs, flat-headed windows and a porch decorated with bargeboard and supported by turned posts. The property has a maintained lawn, mature trees and an established entrance drive with a large setback.</p> <p>History: This property is identified as belonging to J. Crow in the 1862 Tremaine’s Map. “B.F.” is identified as the property owner in the 1880 Historical Atlas map. A house is identified as being in this location in 1880.</p> <p>Context: The property is located on Cream St., which is largely comprised of residential properties set back from the road within a rural/agricultural area.</p>	
BHR 16	1128 Cream St.	Identified during field review	Residential	<p>Design: A 1.5-storey, frame residential building clad in siding, likely built prior to 1880. The building has a T-shaped floorplan with a cross gable roof and flat-headed windows. The entranceway is covered by a porch, and a pointed gable is centred over the porch. The property has a maintained lawn, mature trees and an established entrance drive, with a large setback.</p> <p>History: This property is identified as belonging to J. Crow in the 1862 Tremaine’s Map. “J.S.” is identified as the property owner in the 1880 Historical Atlas map. A house is identified as being in this location in 1880.</p> <p>Context: The property is located on Cream St., which is largely comprised of residential properties set back from the road within a rural/agricultural area.</p>	
BHR 17	1162 Cream St.	Identified during field review	Residential	<p>Design: A 1.5-storey frame residential building clad in siding with a brick-clad addition on the north side, likely built prior to 1880. The building has a rectangular floorplan and symmetrical façade, with a central entranceway flanked by flat-headed windows, a side gable roof and an open verandah supported by turned posts. A shed is located to the north of the house. The property has a maintained lawn, mature plantings and an established entrance drive.</p> <p>History: No property owner is identified in the 1862 Tremaine’s Map or the 1880 Historical Atlas map. A house is identified as being in this location in 1880.</p> <p>Context: The property is located on Cream St., which is largely comprised of residential properties set back from the road within a rural/agricultural area.</p>	

Table 4 Detailed description of built heritage resources (BHR) and cultural heritage landscapes (CHL) in the study area

Feature ID	Address	Heritage Status	Resource Type	Description/Comments	Photograph(s)
BHR 18	578 Welland Rd.	Identified during field review	Residential	<p>Design: A 2-storey frame residential building clad in siding with a square floorplan, a hip roof and an external brick chimney, likely built between 1920 and 1938. The asymmetrical façade features a portico supported by columns, a second-storey balcony, and a gabled dormer. The flat-headed windows have shutters. A garage is located to the east of the house. The property has a maintained lawn, a picket fence, mature plantings and an established entrance drive.</p> <p>History: This property is identified as belonging to J. F. Stitzinger in the 1862 Tremaine’s Map and in the 1880 Historical Atlas map.</p> <p>Context: The property is located near the corner of Cream St. on Welland Rd., which is largely comprised of residential properties and agricultural land.</p>	
BHR 19	630 Welland Rd.	Identified during field review	Residential	<p>Design: A 1.5-storey frame residential building clad in siding, with a single-storey extension on the west side, likely built prior to 1862. The building features a rectangular floor plan and a symmetrical façade. The central entranceway is flanked by flat-headed windows with shutters. The side gable roof features a central gable, and a second-storey window is centred above a porch with a bell-curve roof. A garage is located to the east of the house. The property has a maintained lawn, mature trees and an established entrance drive.</p> <p>History: No property owner is identified in the 1862 Tremaine’s Map. A house is identified in this location in 1862. T. Scanton is identified as the property owner in the 1880 Historical Atlas map.</p> <p>Context: The property is located on Welland Rd., which is largely comprised of residential properties and agricultural land.</p>	
BHR 20	646 Welland Rd.	Identified during field review	Residential, farmscape	<p>Design: A 1.5-storey frame residential building clad in vinyl siding, with a large addition at the rear, likely built prior to 1862. The building features a symmetrical façade with a porch. The side gable roof has a central gable and the central entranceway is flanked by flat-headed windows with shutters. The property has a maintained lawn, mature trees and an established entrance drive. The property contains a field behind the house.</p> <p>History: This property is identified as belonging to U. Rice in the 1862 Tremaine’s Map. A house is identified as being in this location in 1862. D. Leppert is identified as the property owner in the 1880 Historical Atlas map.</p> <p>Context: The property is located on Welland Rd., which is largely comprised of residential properties and agricultural land.</p>	
BHR 21	663 Welland Rd.	Identified during field review	Residential	<p>Design: A one-storey, frame residential building clad in vinyl siding, likely built between 1920 and 1938. The L-shaped floorplan has a cross gable roof. The asymmetrical façade features flat-headed windows. A garage is located northeast of the house. The property has a maintained lawn, mature trees and an established entrance drive.</p> <p>History: This property is identified as belonging to R. Farr in the 1862 Tremaine’s Map and in the 1880 Historical Atlas map.</p> <p>Context: The property is located on Welland Rd., which is largely comprised of residential properties and agricultural land.</p>	